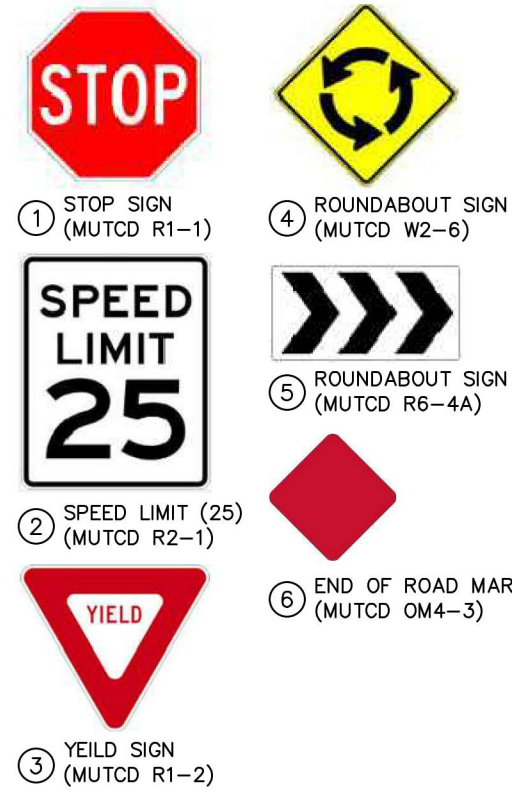


30" VALLEY GUTTER WILL BE USED IN FRONT OF ALL FRONT LOADED SINGLE FAMILY LOTS AND TOWNHOME LOTS.

TOWNHOME NOTE:
TOWNHOME BUILDINGS ARE CONCEPTUAL AND WILL BE APPROVED SEPARATE FROM THESE PLANS.



PAVEMENT MARKING LEGEND

(A) 24" WIDE STOP BAR

(B) 8" WIDE STANDARD CROSSWALK (8" WHITE LINE)

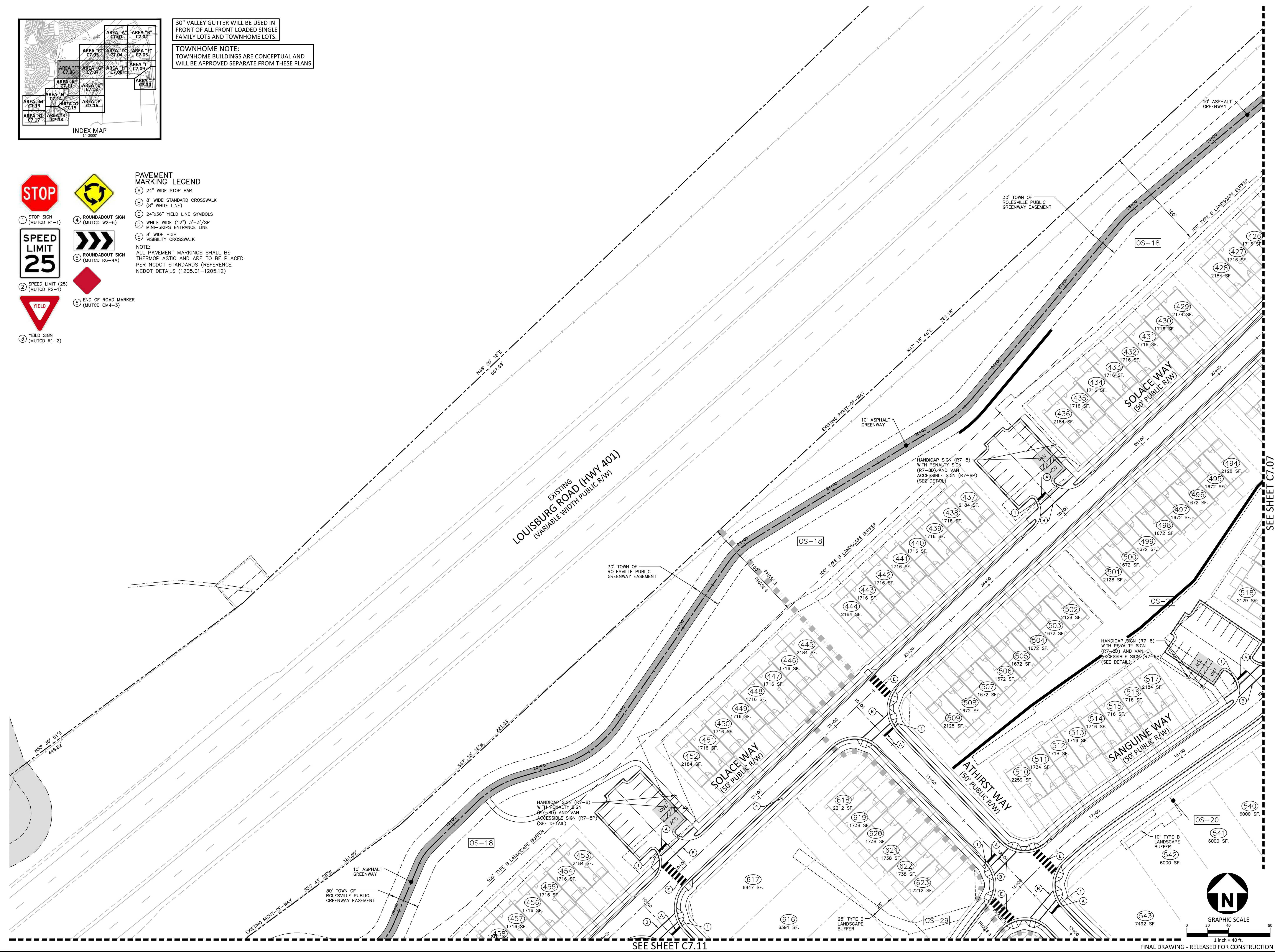
(C) 24"x36" YIELD LINE SYMBOLS

(D) WHITE WIDE (12") 3"-3"/SP MINI-SKIPS ENTRANCE LINE

(E) 8" WIDE HIGH VISIBILITY CROSSWALK

NOTE:
ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC AND ARE TO BE PLACED PER NCDOT STANDARDS (REFERENCE NCDOT DETAILS (1205.01-1205.12)

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PHONE: 919. 422. 7663
CONTACT: BOB MISHLER

ASHTON WOODS.

THE POINT

PHASES 3, 4, 5, 7 AND 10

CONSTRUCTION DRAWINGS - PACKAGE 2

EAST YOUNG STREET

TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08



William T. O'Daniel
c/o-William T. O'Daniel, c=US,
o=North Carolina,
email=rodaniel@mcadamsco.com
2023.05.05 15:23:28 -0400

REVISIONS

NO.	DATE

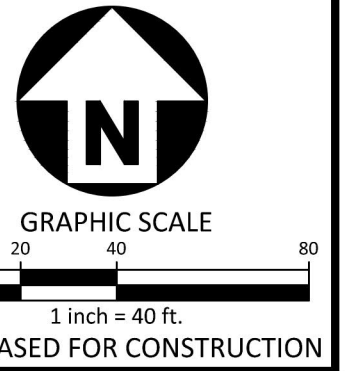
PLAN INFORMATION

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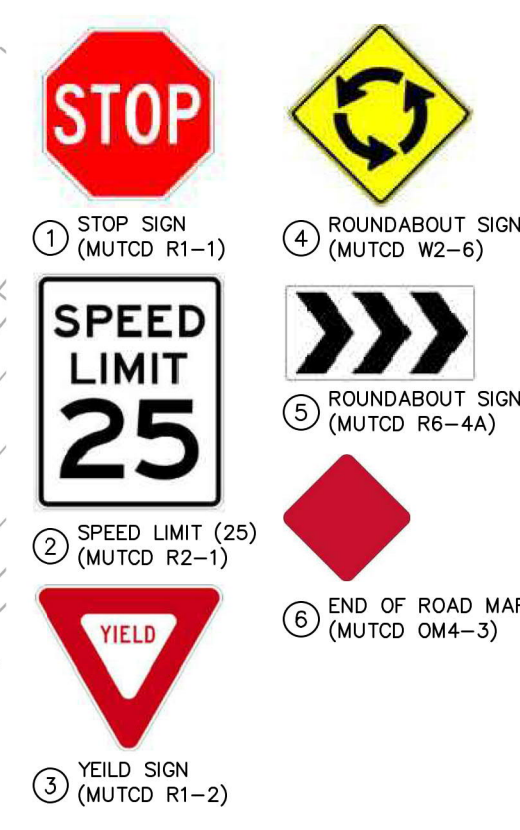
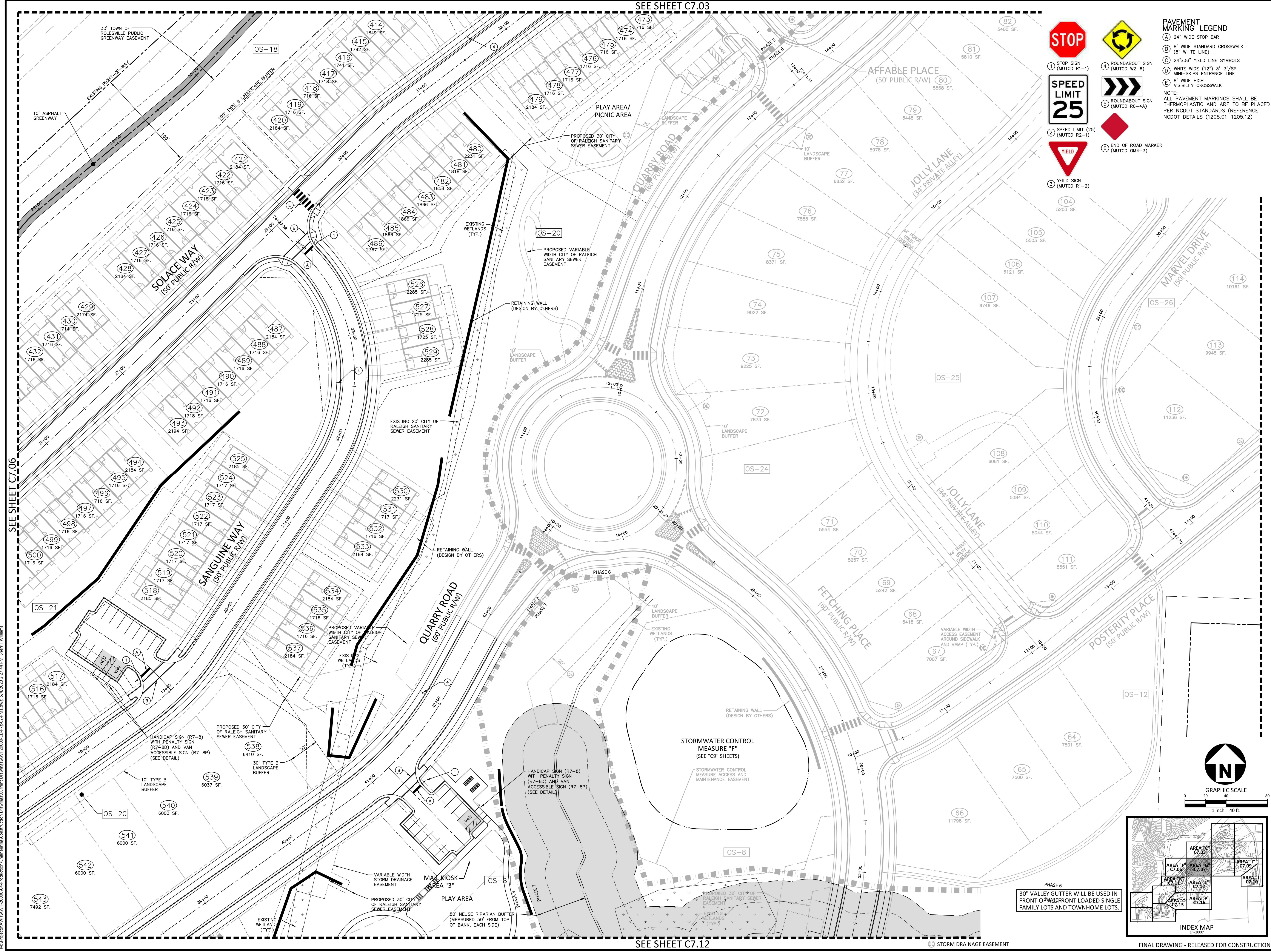
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PAVEMENT MARKING AND SIGNAGE PLAN AREA "F"

C7.06



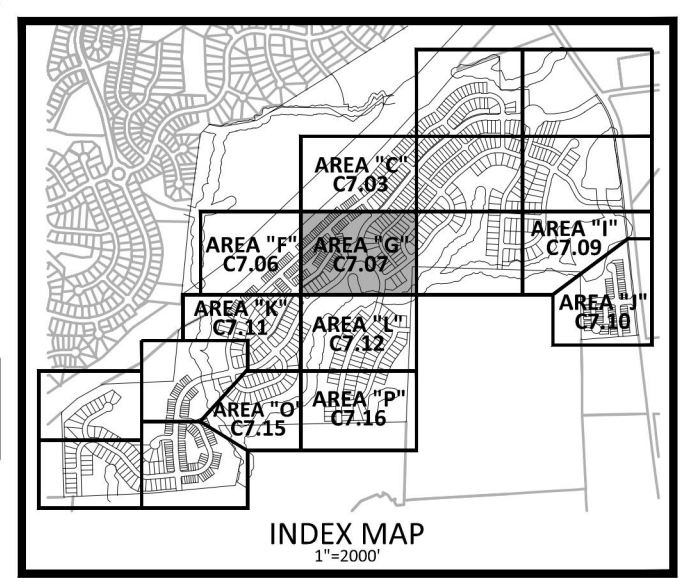
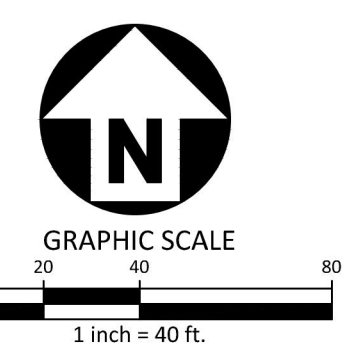
FINAL DRAWING - RELEASED FOR CONSTRUCTION



PAVEMENT MARKING LEGEND

- (A) 24" WIDE STOP BAR
- (B) 8" WIDE STANDARD CROSSWALK (6" WHITE LINE)
- (C) 24"x36" YIELD LINE SYMBOLS
- (D) WHITE WIDE (12") 3"-3"/SP MINI-SKIPS ENTRANCE LINE
- (E) 8" WIDE HIGH VISIBILITY CROSSWALK

NOTE: ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC AND ARE TO BE PLACED PER NCDOT STANDARDS (REFERENCE NCDOT DETAILS (1205.01-1205.12))



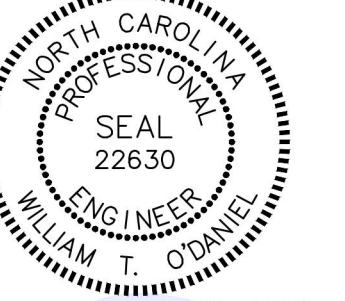
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ASHTON WOODS

THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08



William T. O'Daniel
Professional Engineer
No. 22630
2023.05.05 15:23:44 -04'00'

REVISIONS

NO.	DATE

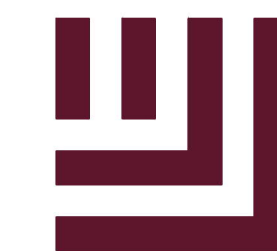
PLAN INFORMATION

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FILENAME AWH20000-CD-PKG-02-PM1
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DRAWN BY .
SCALE 1"=40'
DATE 05.08.2023

PAVEMENT MARKING AND SIGNAGE PLAN AREA "G"

C7.07

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ASHTON WOODS.

**THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET**

TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08



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c/o William T. O'Daniel, c/o US
North Carolina
email: nodaniel@mcadamsco.com
2023.05.05 15:23:58 -0400

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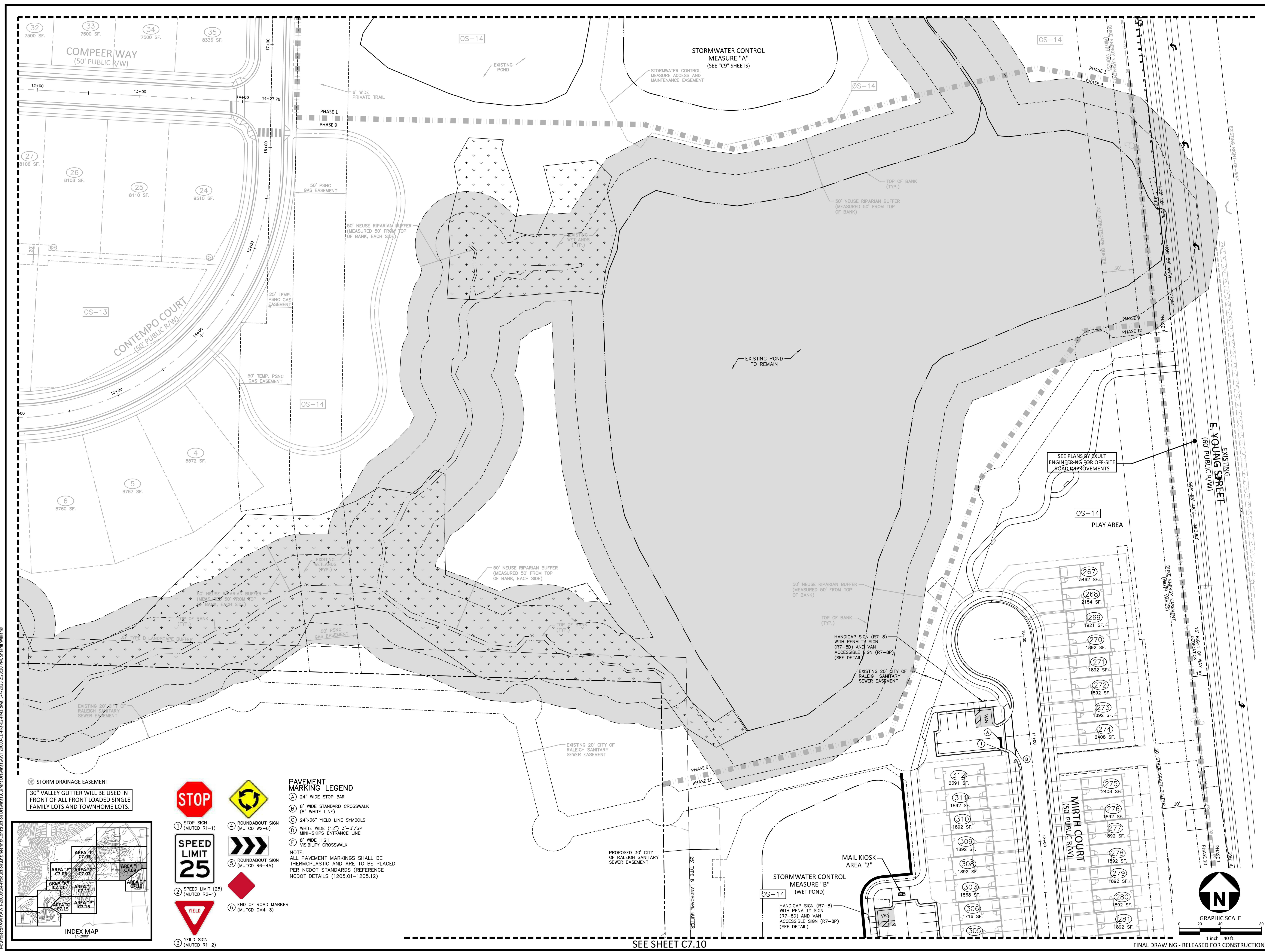
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CHECKED BY	
DRAWN BY	
SCALE	1"=40'
DATE	05.08.2023

SHEET

PAVEMENT MARKING AND SIGNAGE PLAN
AREA "1"

C7.09



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STOP
1 STOP SIGN (MUTCD R1-1)

SPEED LIMIT 25
2 SPEED LIMIT (25) (MUTCD R2-1)

YIELD
3 YIELD SIGN (MUTCD R1-2)

ROUNDABOUT SIGN
4 ROUNDABOUT SIGN (MUTCD W2-6)

ROUNDABOUT SIGN
5 ROUNDABOUT SIGN (MUTCD R6-4A)

END OF ROAD MARKER
6 END OF ROAD MARKER (MUTCD OM4-3)

PAVEMENT MARKING LEGEND

A 24" WIDE STOP BAR

B 8" WIDE STANDARD CROSSWALK (8" WHITE LINE)

C 24"x36" YIELD LINE SYMBOLS

D WHITE WIDE (12") 3"-3"/SP MINI-SKIPS ENTRANCE LINE

E 8" WIDE HIGH VISIBILITY CROSSWALK

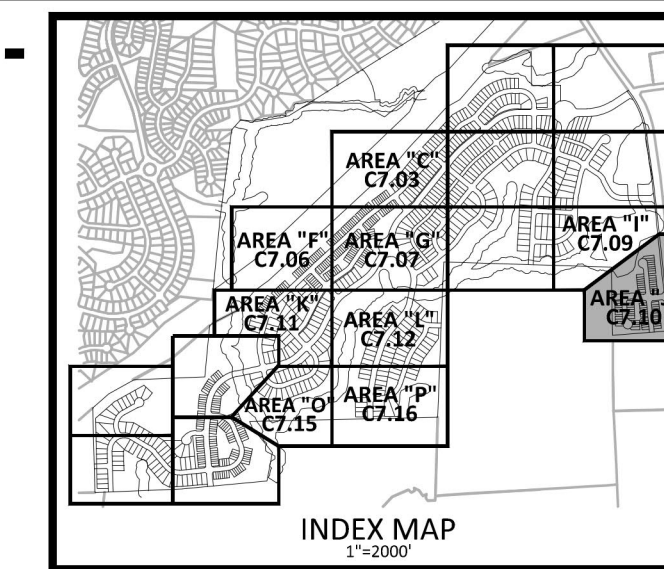
NOTE: ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC AND ARE TO BE PLACED PER NCDOT STANDARDS (REFERENCE NCDOT DETAILS (1205.01-1205.12))

INDEX MAP
1"=2000'

SEE SHEET C7.10

GRAPHIC SCALE
1 inch = 40 ft.

FINAL DRAWING - RELEASED FOR CONSTRUCTION



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THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08



William T. O'Daniel
c/o William T. O'Daniel, c.e.us.
c/o North Carolina,
email: rodaniel@mcadamsco.com
2023.05.05 15:24:10 -0400'

REVISIONS

NO. DATE

PLAN INFORMATION

PROJECT NO. AWH-20000
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CHECKED BY
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SCALE 1"=40'
DATE 05.08.2023

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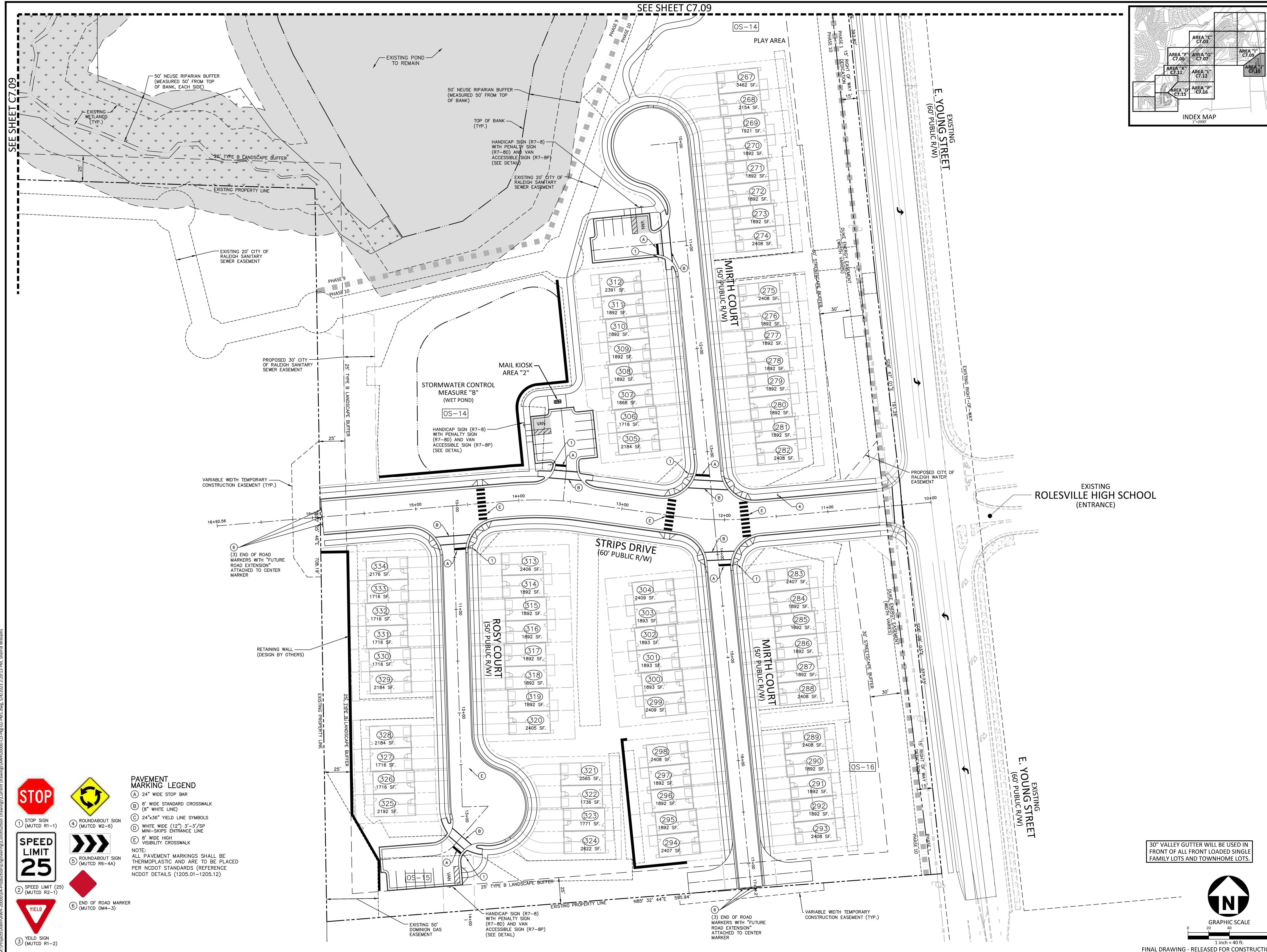
PAVEMENT MARKING AND SIGNAGE PLAN
AREA "J"

C7.10

30" VALLEY GUTTER WILL BE USED IN FRONT OF ALL FRONT LOADED SINGLE FAMILY LOTS AND TOWNHOME LOTS.



FINAL DRAWING - RELEASED FOR CONSTRUCTION

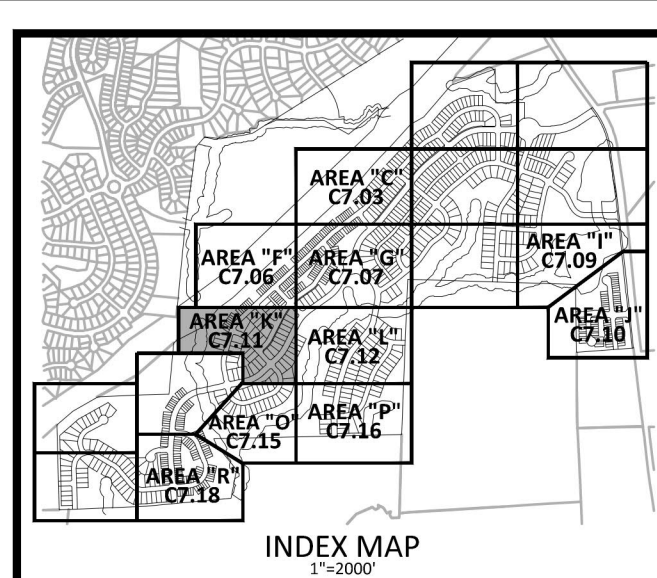


PAVEMENT MARKING LEGEND

- (A) 24" WIDE STOP BAR
 - (B) 8" WIDE STANDARD CROSSWALK (8" WHITE LINE)
 - (C) 24"x36" YIELD LINE SYMBOLS
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 - (E) 8" WIDE HIGH VISIBILITY CROSSWALK
- NOTE:
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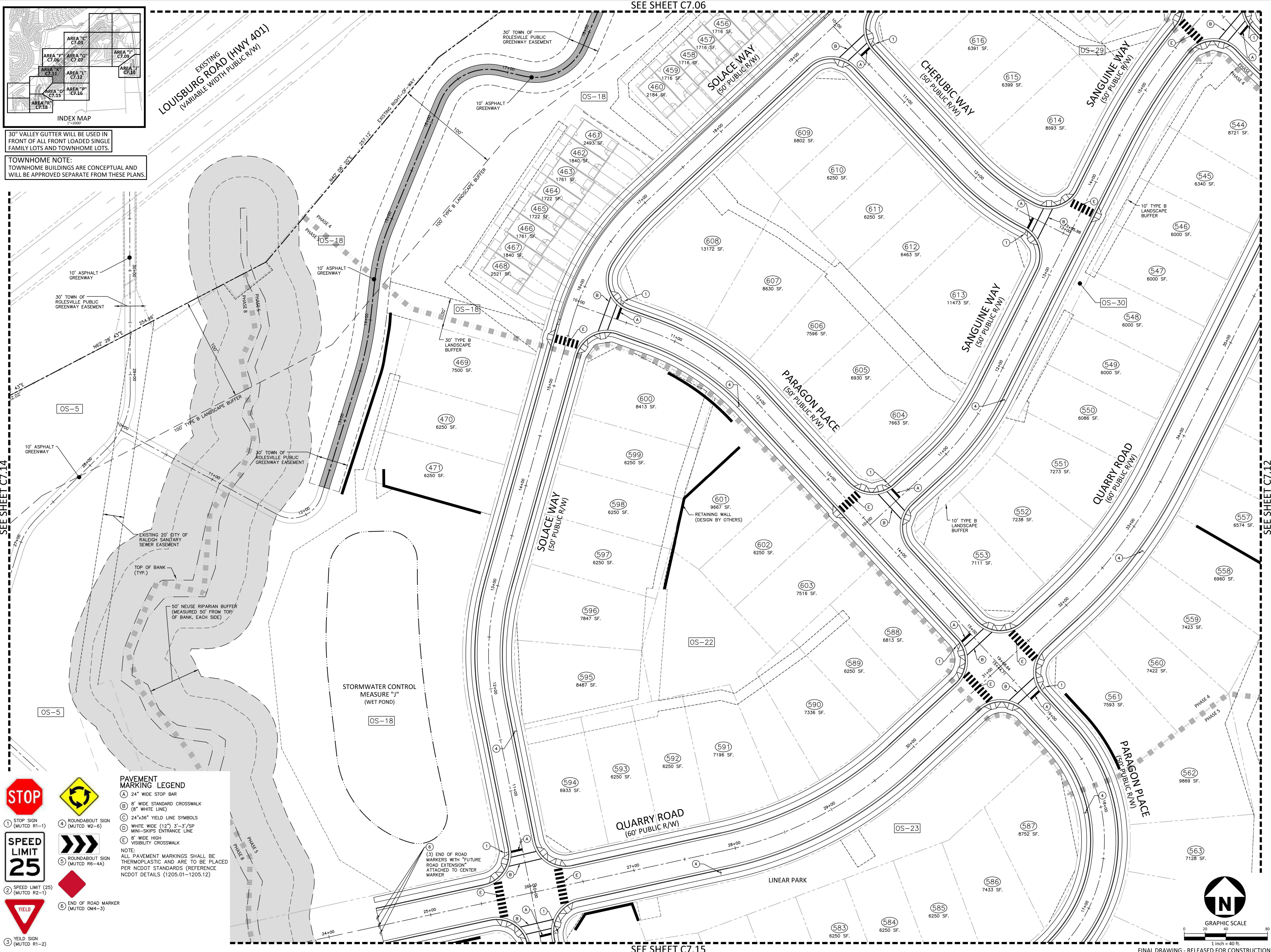
- (1) STOP SIGN (MUTCD R1-1)
- (2) SPEED LIMIT (25) (MUTCD R2-1)
- (3) YIELD SIGN (MUTCD R1-2)
- (4) ROUNDABOUT SIGN (MUTCD W2-6)
- (5) ROUNDABOUT SIGN (MUTCD R6-4A)
- (6) END OF ROAD MARKER (MUTCD O4-3)

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30" VALLEY GUTTER WILL BE USED IN FRONT OF ALL FRONT LOADED SINGLE FAMILY LOTS AND TOWNHOME LOTS.

TOWNHOME NOTE: TOWNHOME BUILDINGS ARE CONCEPTUAL AND WILL BE APPROVED SEPARATE FROM THESE PLANS.



PAVEMENT MARKING LEGEND

- (A) 24" WIDE STOP BAR
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- (D) WHITE WIDE (12") 3'-3"/SP MINI-SKIPS ENTRANCE LINE
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NOTE: ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC AND ARE TO BE PLACED PER NCDOT STANDARDS (REFERENCE NCDOT DETAILS 1205.01-1205.12)

(3) END OF ROAD MARKERS WITH "FUTURE ROAD EXTENSION" ATTACHED TO CENTER MARKER

STOP
1 STOP SIGN (MUTCD R1-1)

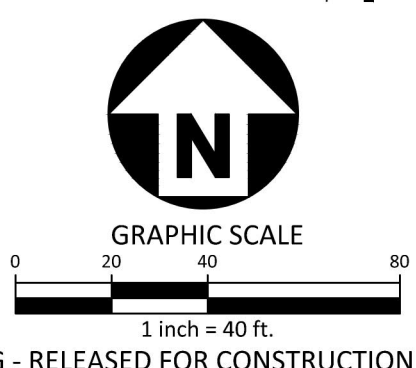
SPEED LIMIT 25
2 SPEED LIMIT (25) (MUTCD R2-1)

YIELD
3 YIELD SIGN (MUTCD R1-2)

ROUNDABOUT SIGN
4 ROUNDABOUT SIGN (MUTCD W2-6)

ROUNDABOUT SIGN
5 ROUNDABOUT SIGN (MUTCD R6-4A)

END OF ROAD MARKER
6 END OF ROAD MARKER (MUTCD O4-3)



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SEE SHEET C7.06

SEE SHEET C7.15

SEE SHEET C7.12

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ASHTON WOODS.

THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08

W.T. O'Daniel

REVISIONS

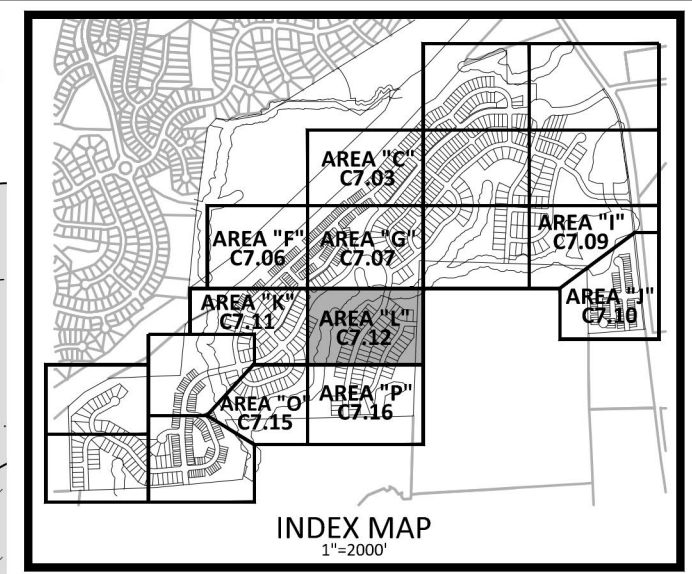
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FILENAME AWH20000-CD-PKG-02-PML1
CHECKED BY
DRAWN BY
SCALE 1"=40'
DATE 05.08.2023

SHEET
PAVEMENT MARKING AND SIGNAGE PLAN
AREA "K"
C7.11

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ASHTON WOODS

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 TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
 WAKE COUNTY, NORTH CAROLINA

CD 20-08



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REVISIONS

NO.	DATE

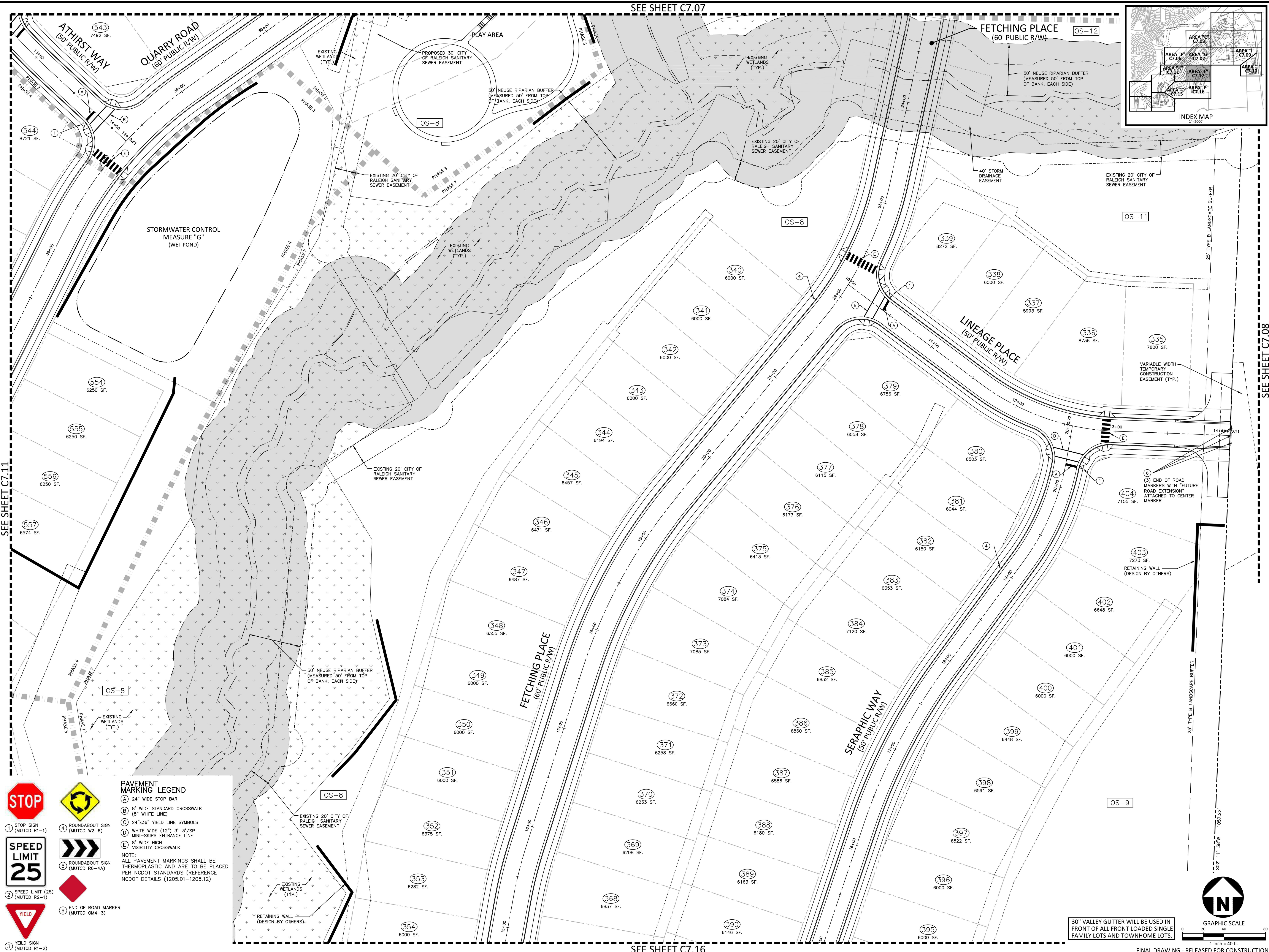
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DATE	05.08.2023

SHEET

PAVEMENT MARKING AND SIGNAGE PLAN AREA "I"

C7.12



SEE SHEET C7.11

SEE SHEET C7.08

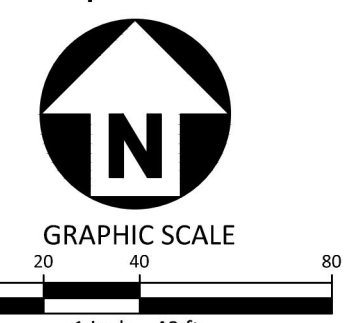
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STOP
 1 STOP SIGN (MUTCD R1-1)
SPEED LIMIT 25
 2 SPEED LIMIT (25) (MUTCD R2-1)
YIELD
 3 YIELD SIGN (MUTCD R1-2)

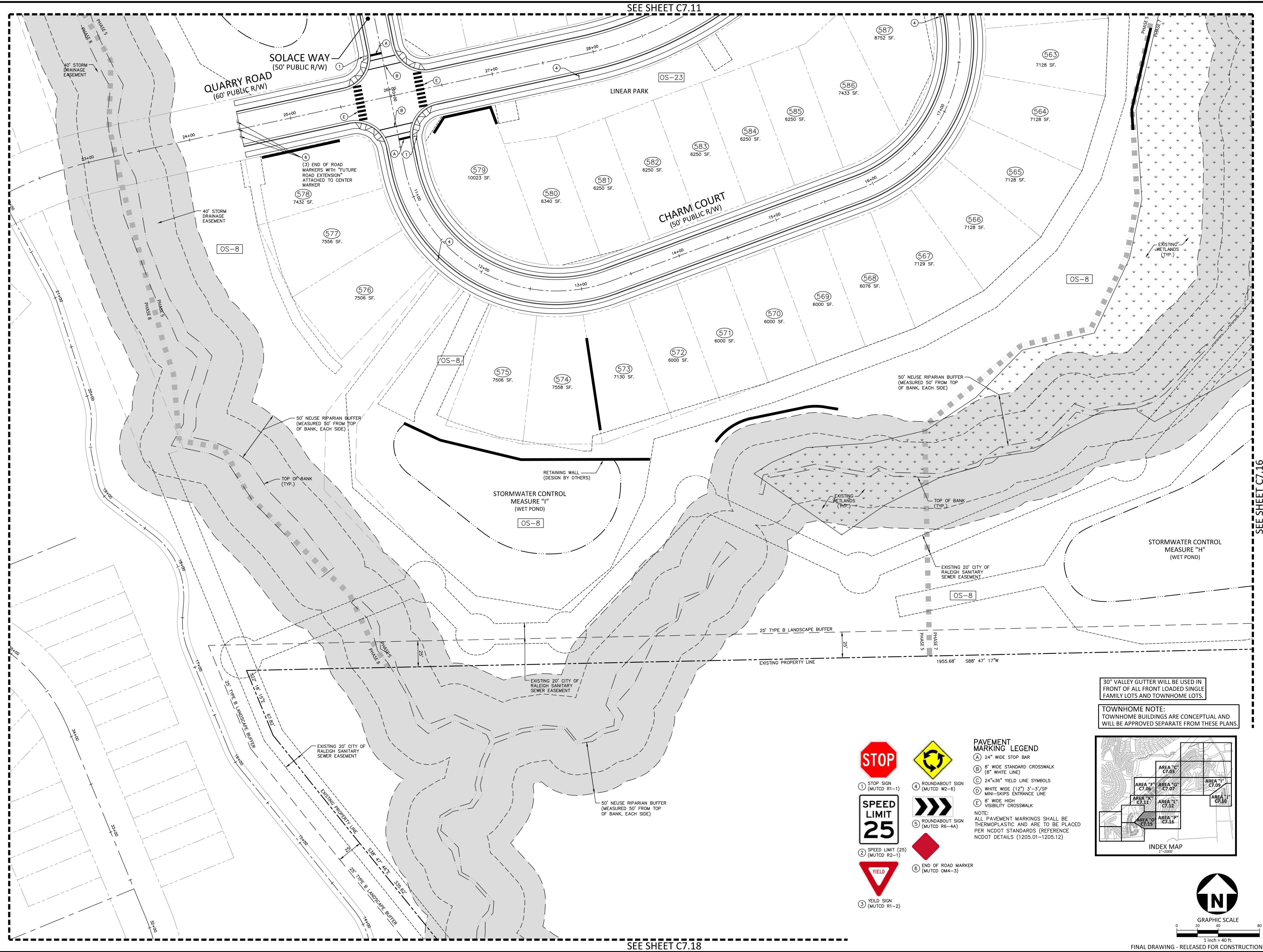
PAVEMENT MARKING LEGEND
 A 24" WIDE STOP BAR
 B 8" WIDE STANDARD CROSSWALK (8" WHITE LINE)
 C 24"x36" YIELD LINE SYMBOLS
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NOTE:
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SEE SHEET C7.11



SEE SHEET C7.18

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ASHTON WOODS

THE POINT
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CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08



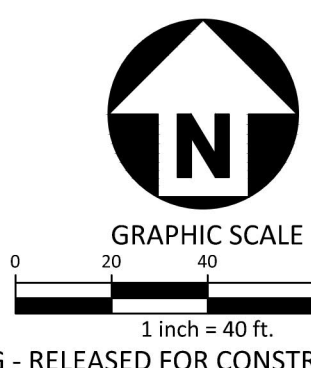
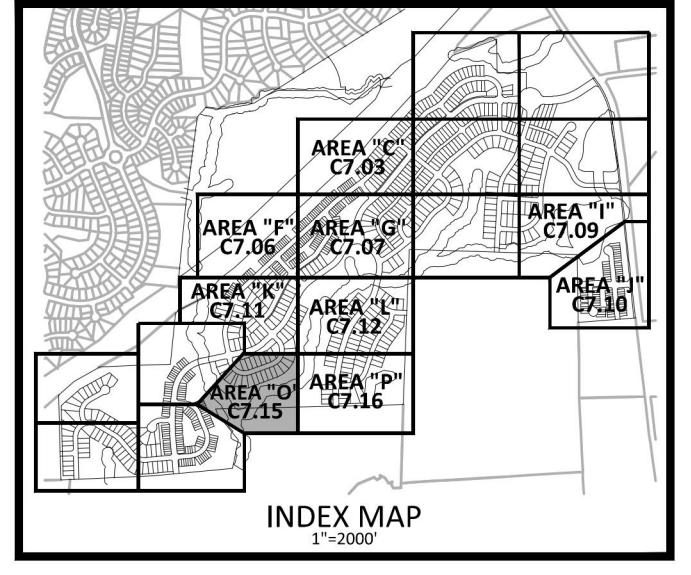
William T. O'Daniel
c=William T O'Daniel, c=US,
o=North Carolina,
email=rodaniel@mcadamsco.com
2023.05.05 15:25:05 -04'00'

30" VALLEY GUTTER WILL BE USED IN FRONT OF ALL FRONT LOADED SINGLE FAMILY LOTS AND TOWNHOME LOTS.

TOWNHOME NOTE:
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- 1 STOP SIGN (MUTCD R1-1)
- 2 SPEED LIMIT (25) (MUTCD R2-1)
- 3 YIELD SIGN (MUTCD R1-2)
- 4 ROUNDABOUT SIGN (MUTCD W2-6)
- 5 ROUNDABOUT SIGN (MUTCD R6-4A)
- 6 END OF ROAD MARKER (MUTCD DM4-3)

PAVEMENT MARKING LEGEND
 (A) 24" WIDE STOP BAR
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 (D) WHITE WIDE (12") 3"-3"/3P MINI-SKIPS ENTRANCE LINE
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 NOTE:
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REVISIONS

NO.	DATE

PLAN INFORMATION

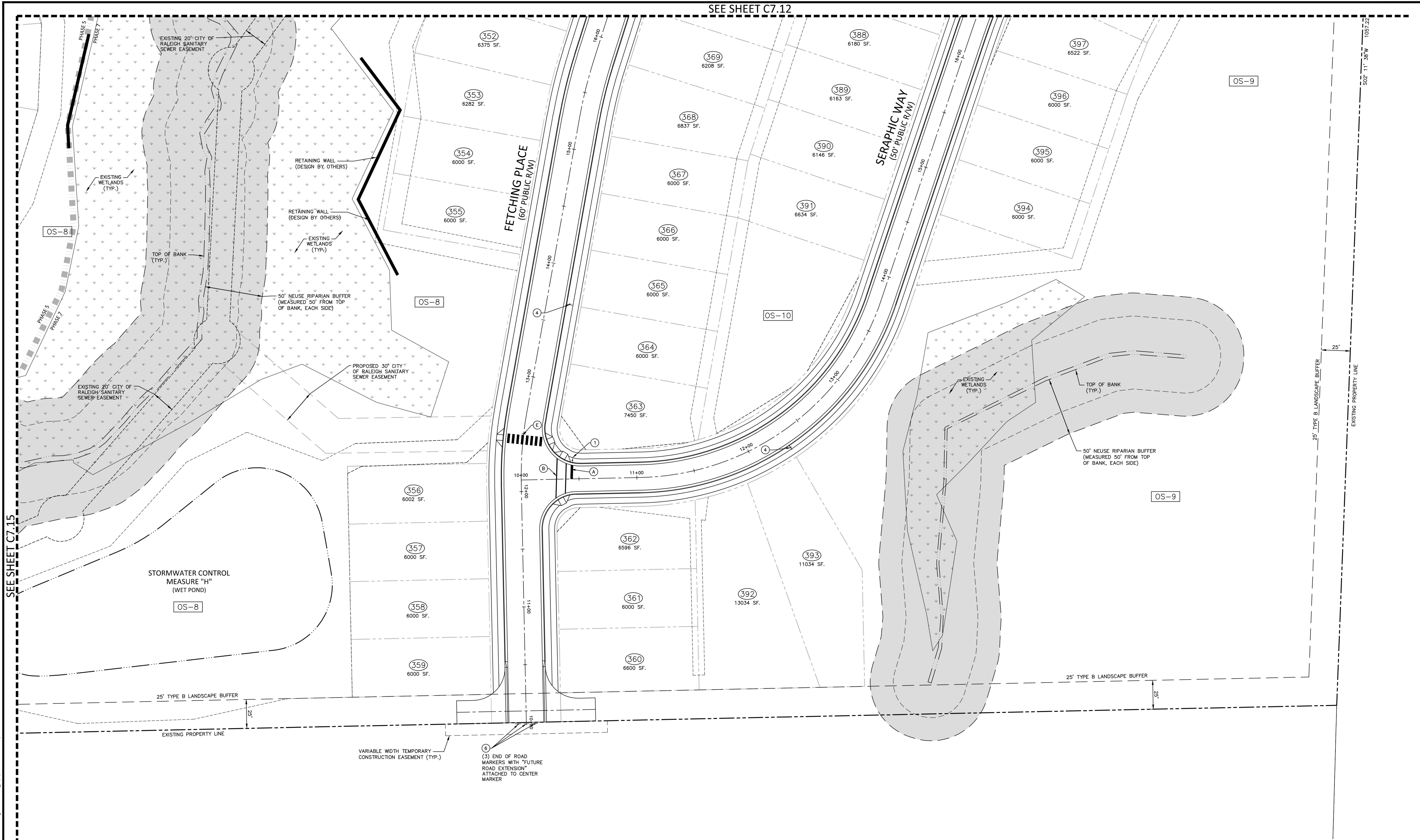
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 DATE 05.08.2023

SHEET

PAVEMENT MARKING AND SIGNAGE PLAN AREA "O"
C7.15

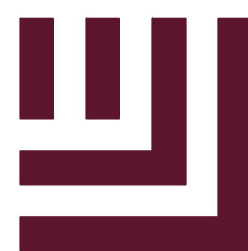
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SEE SHEET C7.15

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ASHTON WOODS.

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TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
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CD 20-08



William T. O'Daniel
c=William T. O'Daniel, c=US,
o=North Carolina,
e=odaniel@mcadamsco.com
2023.05.05 15:26:45 -0400

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NO. DATE

PLAN INFORMATION

PROJECT NO. AWH-20000
FILENAME AWH20000-CD-PKG-02-PKG-02-P1
CHECKED BY
DRAWN BY
SCALE 1"=40'
DATE 05.08.2023

SHEET

PAVEMENT MARKING
AND SIGNAGE PLAN
AREA "P"

C7.16



1 STOP SIGN (MUTCD R1-1)



2 SPEED LIMIT (25) (MUTCD R2-1)



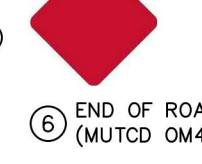
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4 ROUNDABOUT SIGN (MUTCD W2-6)



5 ROUNDABOUT SIGN (MUTCD R6-4A)



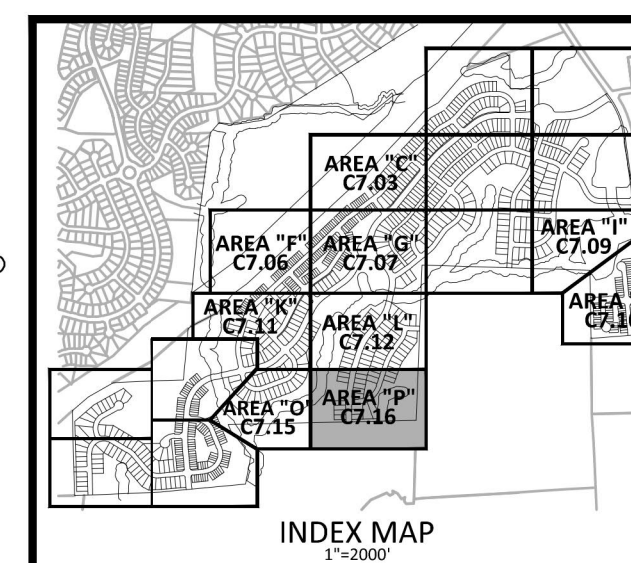
6 END OF ROAD MARKER (MUTCD 0M4-3)

PAVEMENT MARKING LEGEND

- (A) 24" WIDE STOP BAR
- (B) 8" WIDE STANDARD CROSSWALK (8" WHITE LINE)
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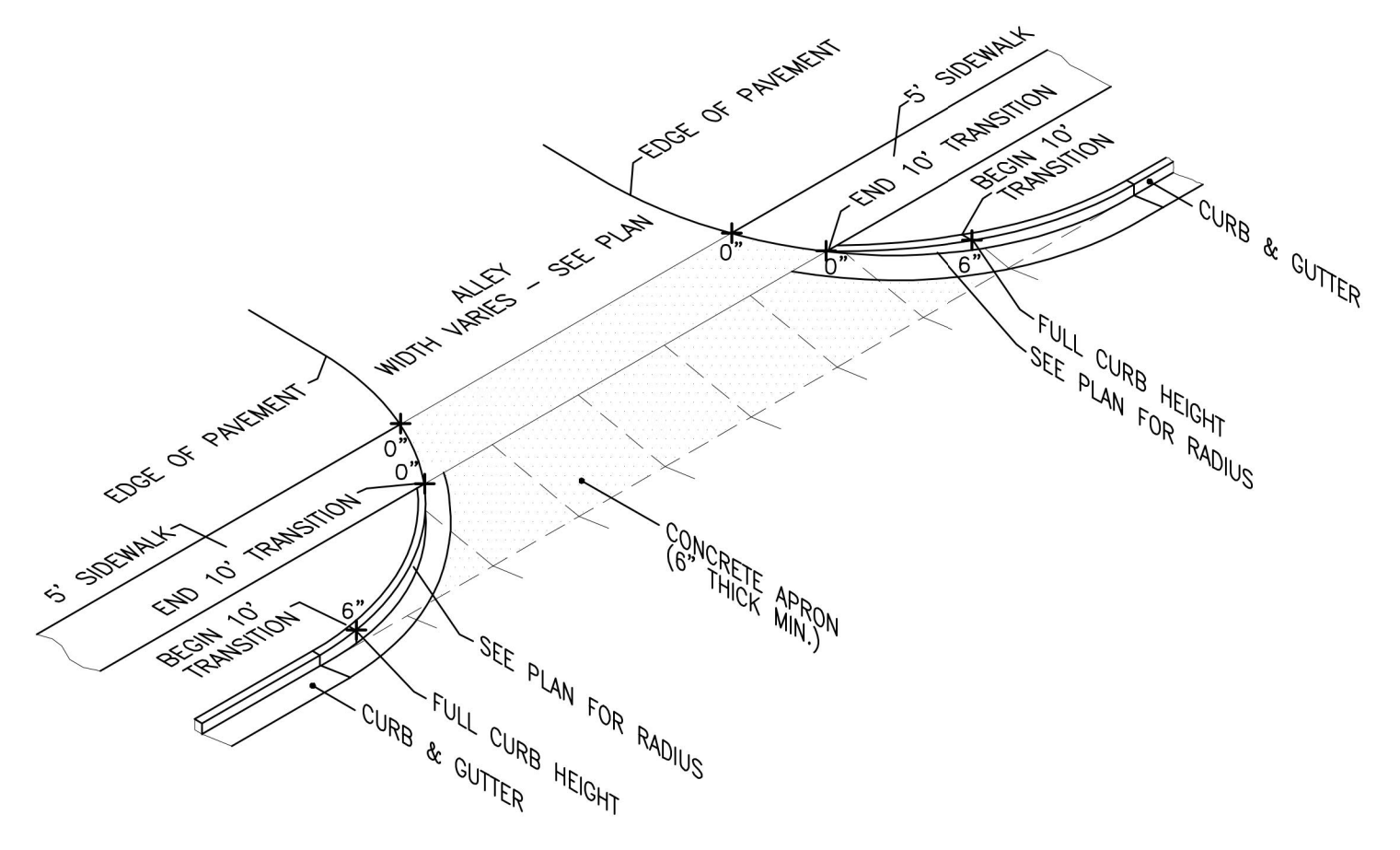
NOTE:
ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC AND ARE TO BE PLACED PER NCDOT STANDARDS (REFERENCE NCDOT DETAILS (1205.01-1205.12)

30" VALLEY GUTTER WILL BE USED IN FRONT OF ALL FRONT LOADED SINGLE FAMILY LOTS AND TOWNHOME LOTS.

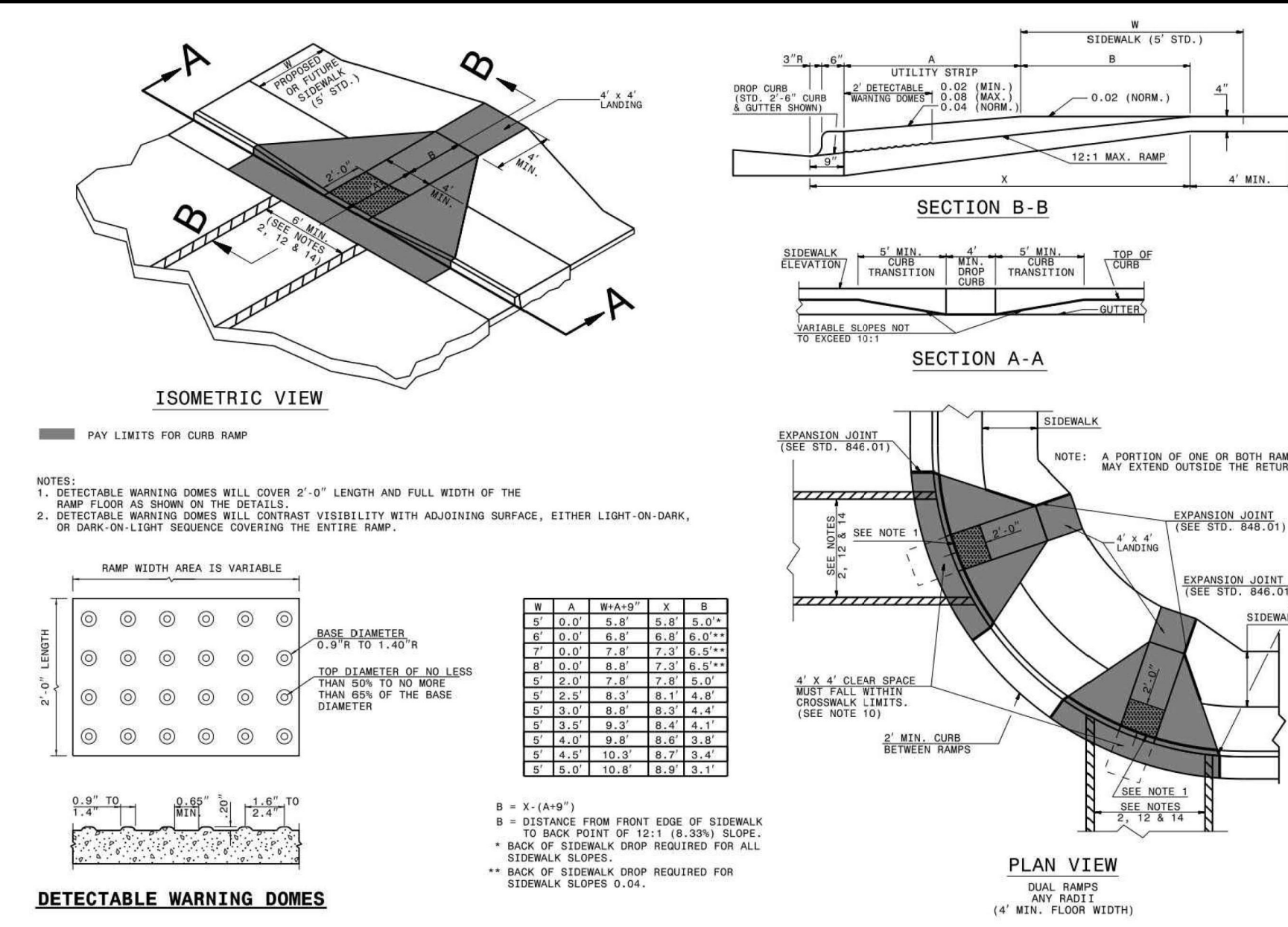


GRAPHIC SCALE
1 inch = 40 ft.

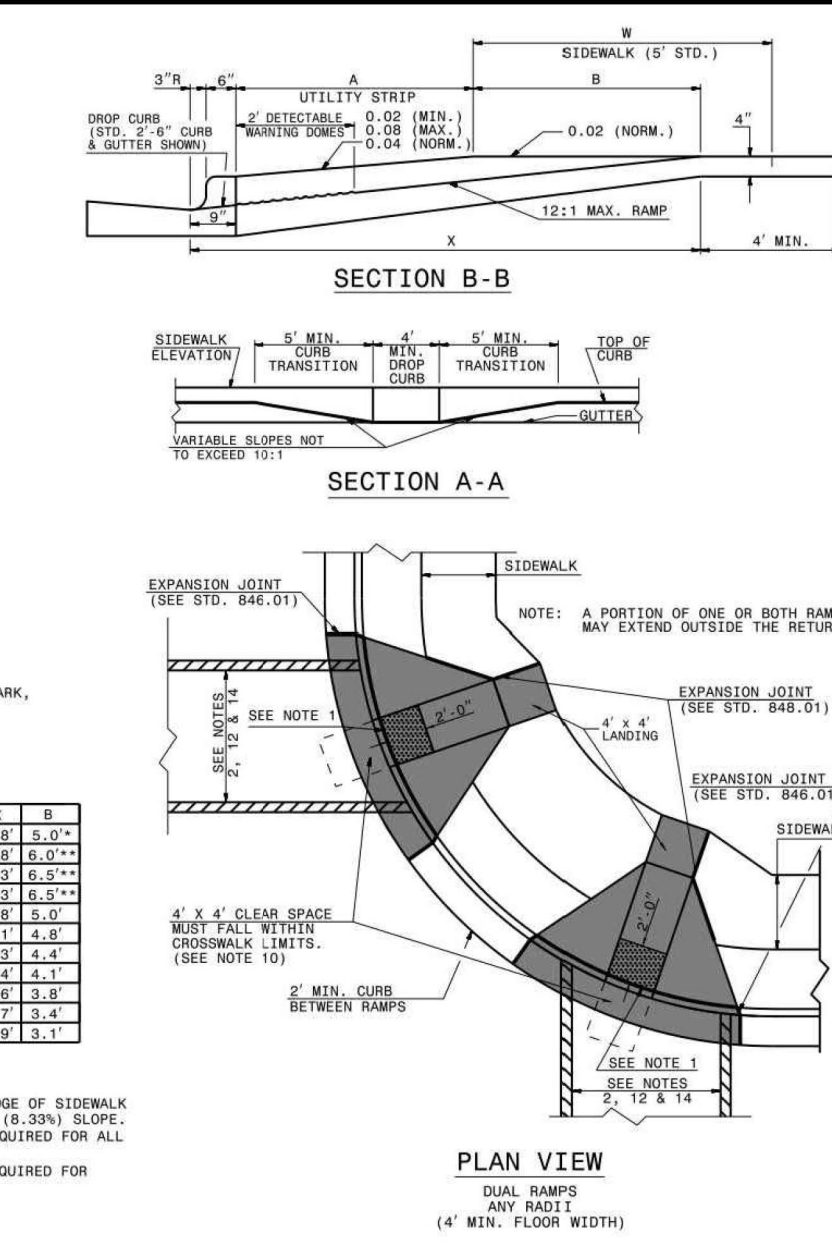
FINAL DRAWING - RELEASED FOR CONSTRUCTION



RESIDENTIAL ALLEY ENTRANCE DETAIL
N.T.S.



DETECTABLE WARNING DOMES



STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

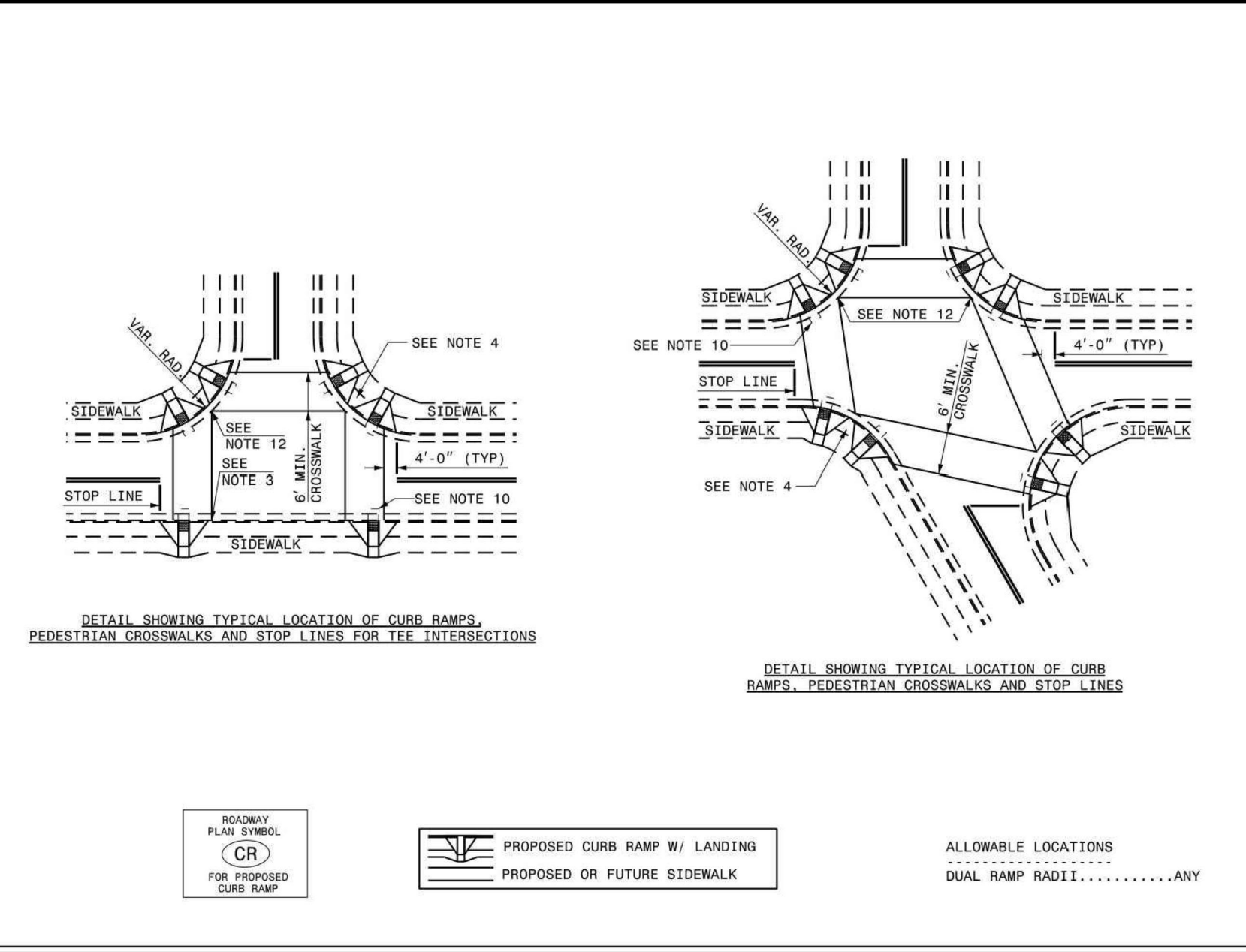
ROADWAY STANDARD DRAWING FOR
CURB RAMP AND GUTTER

PROPOSED CURB RAMP W/ LANDING

PROPOSED OR FUTURE SIDEWALK

ALLOWABLE LOCATIONS
DUAL RAMP RADII: ANY

SHEET 2 OF 3
848.05



STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

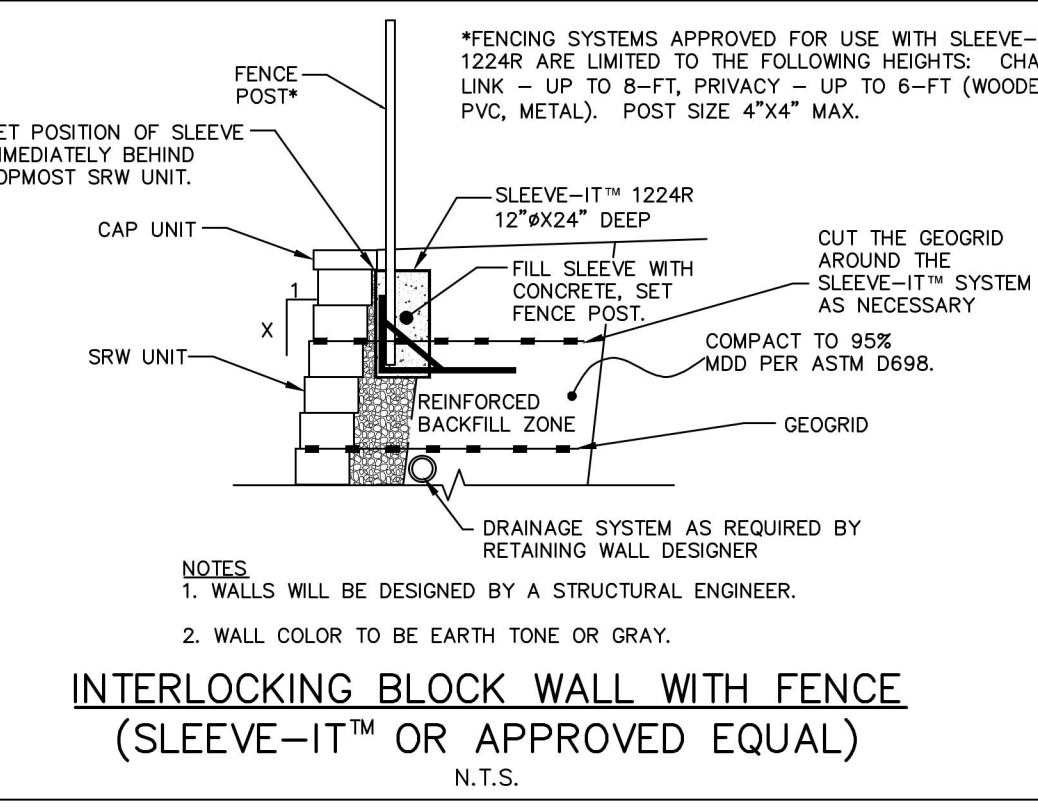
ROADWAY STANDARD DRAWING FOR
CURB RAMP AND GUTTER

PROPOSED CURB RAMP W/ LANDING

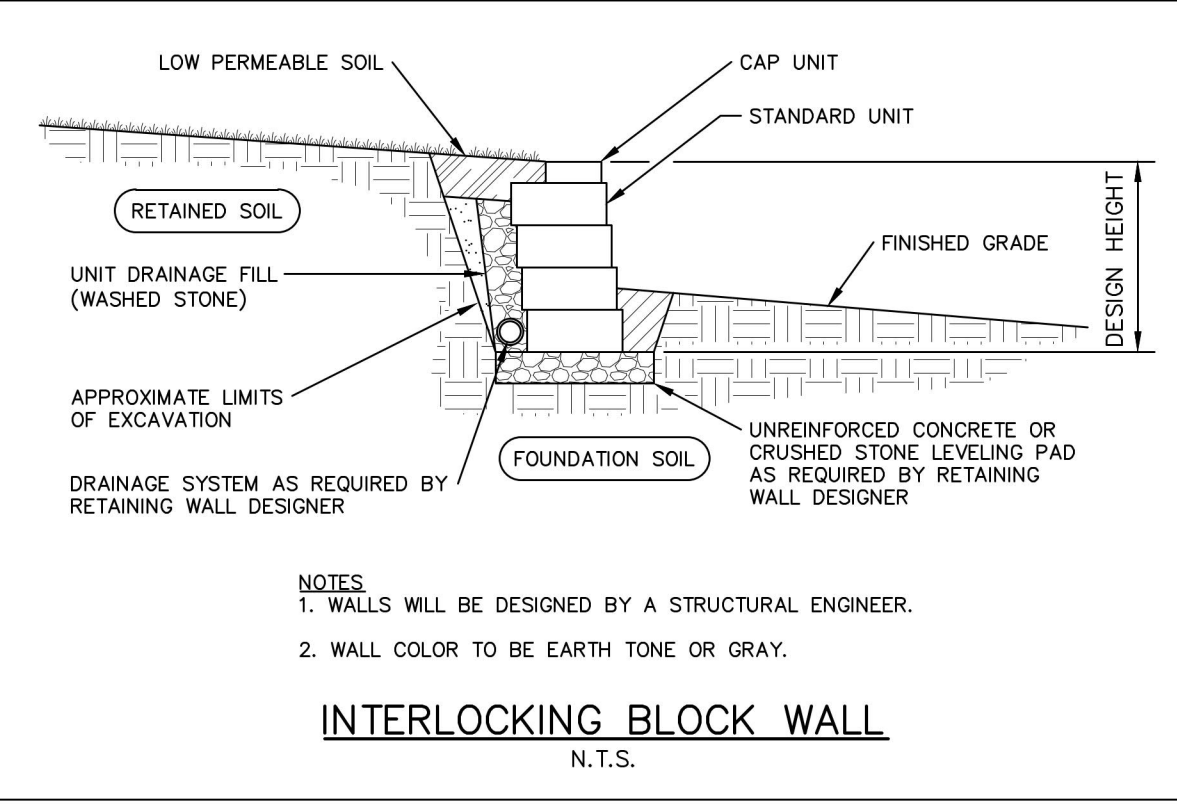
PROPOSED OR FUTURE SIDEWALK

ALLOWABLE LOCATIONS
DUAL RAMP RADII: ANY

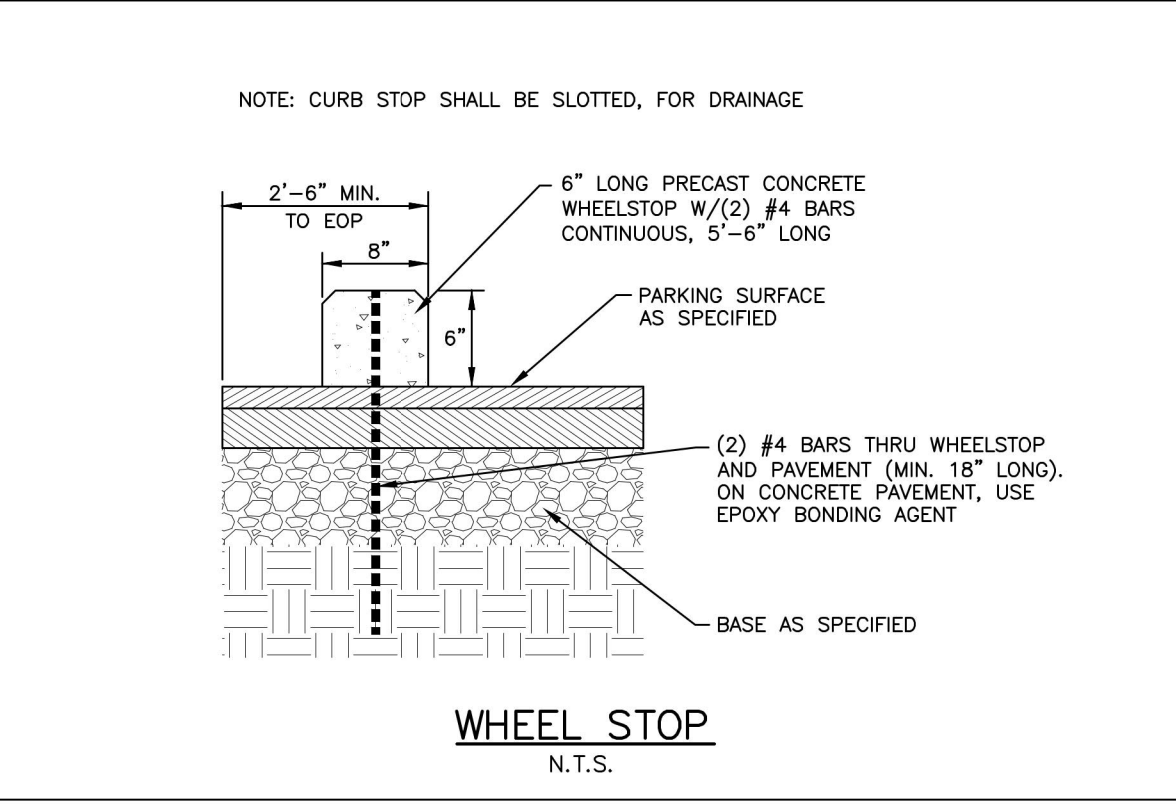
SHEET 2 OF 3
848.05



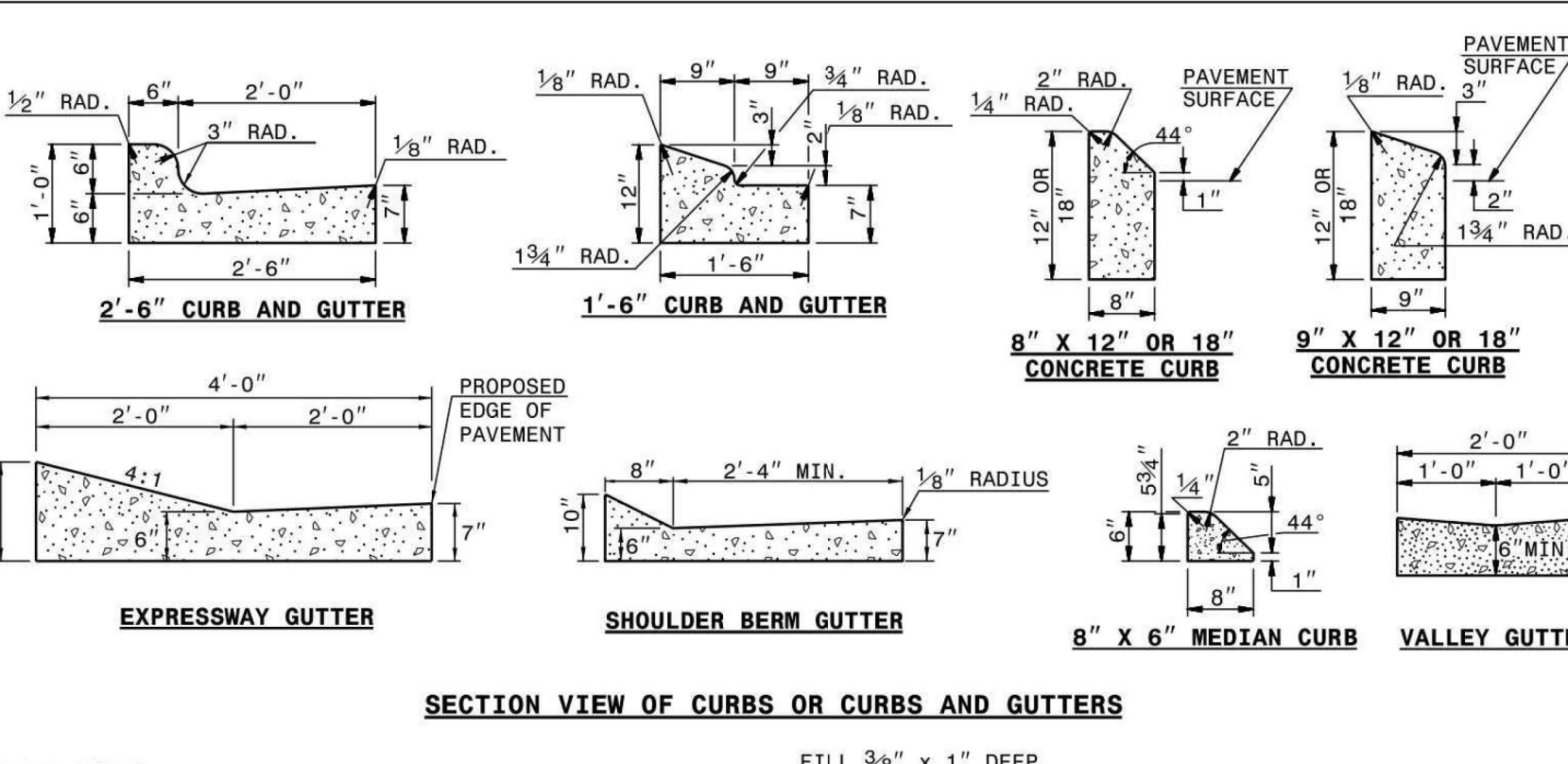
INTERLOCKING BLOCK WALL WITH FENCE (SLEEVE-IT™ OR APPROVED EQUAL)
N.T.S.



INTERLOCKING BLOCK WALL
N.T.S.



WHEEL STOP
N.T.S.

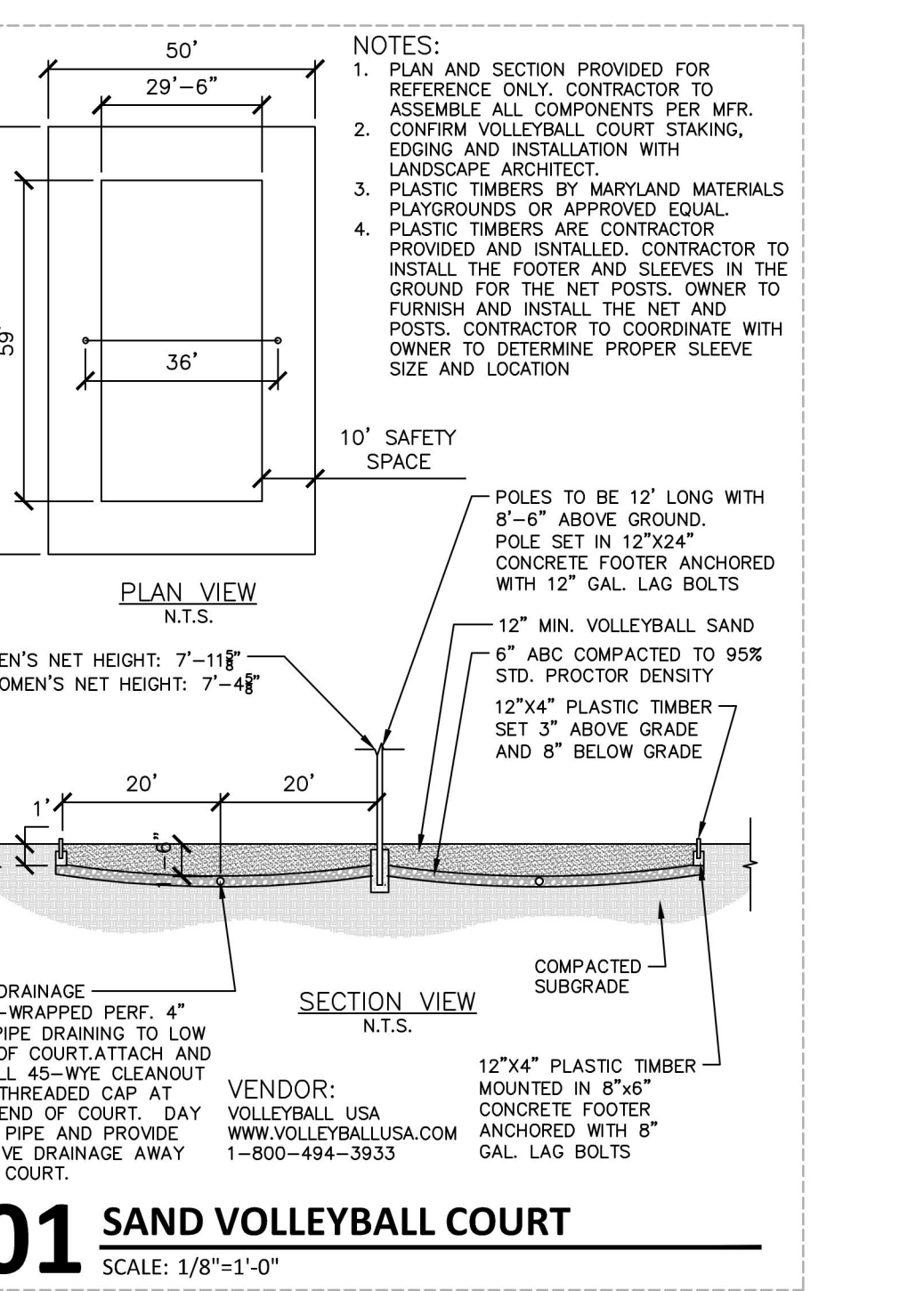


SECTION VIEW OF CURBS OR CURBS AND GUTTERS

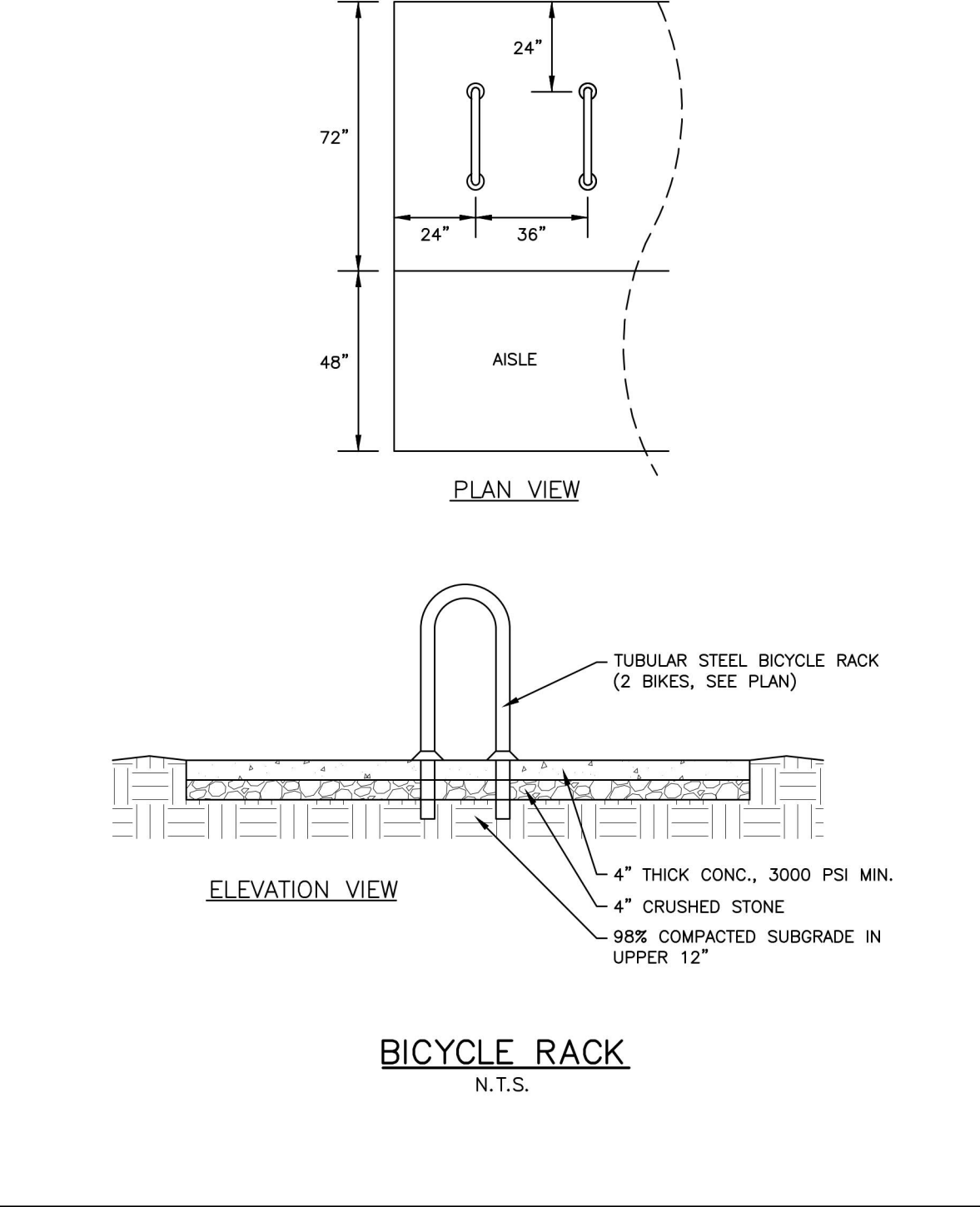
STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAY STANDARD DRAWING FOR
CONCRETE CURB, GUTTER AND CURB & GUTTER

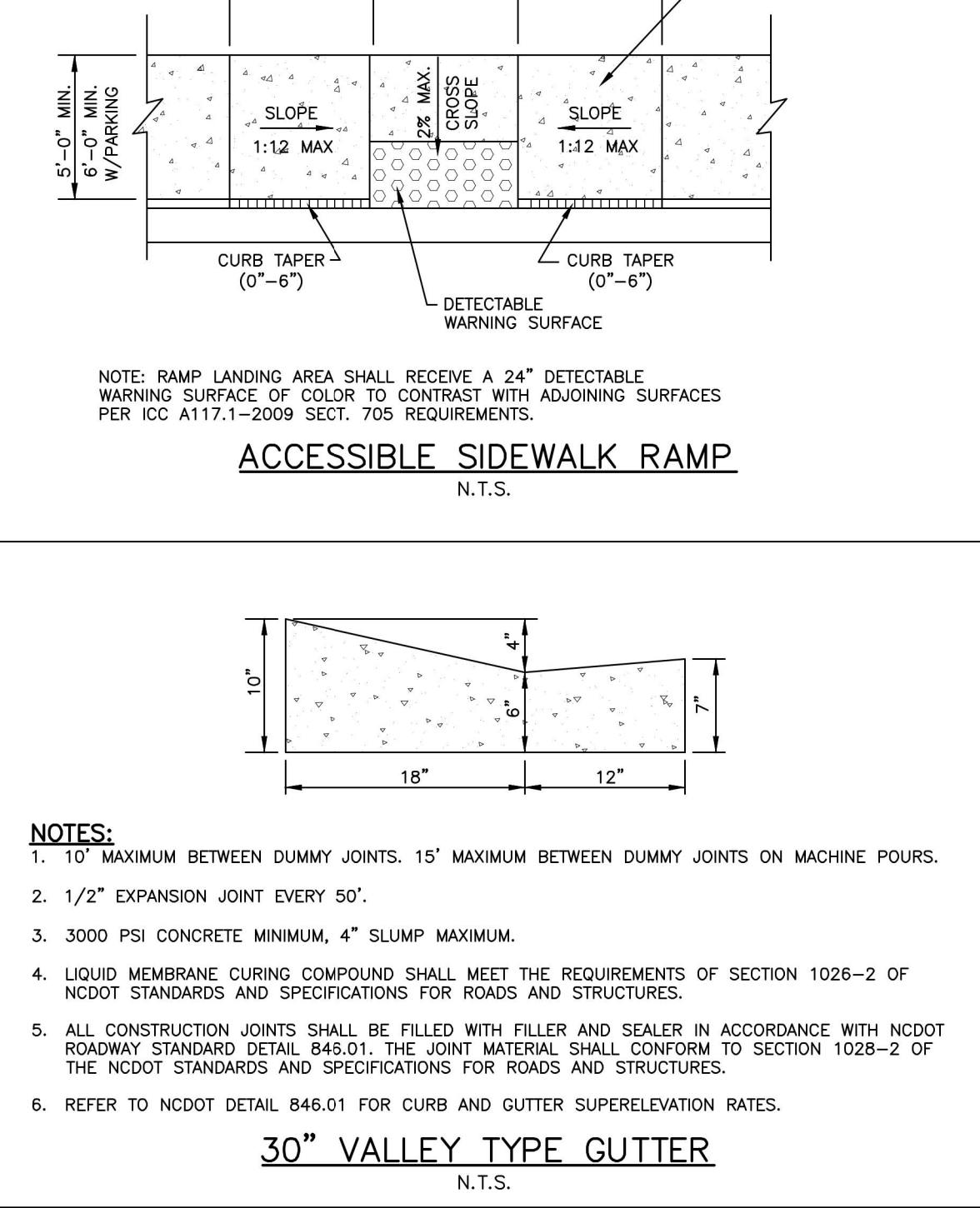
SHEET 1 OF 3
846.01



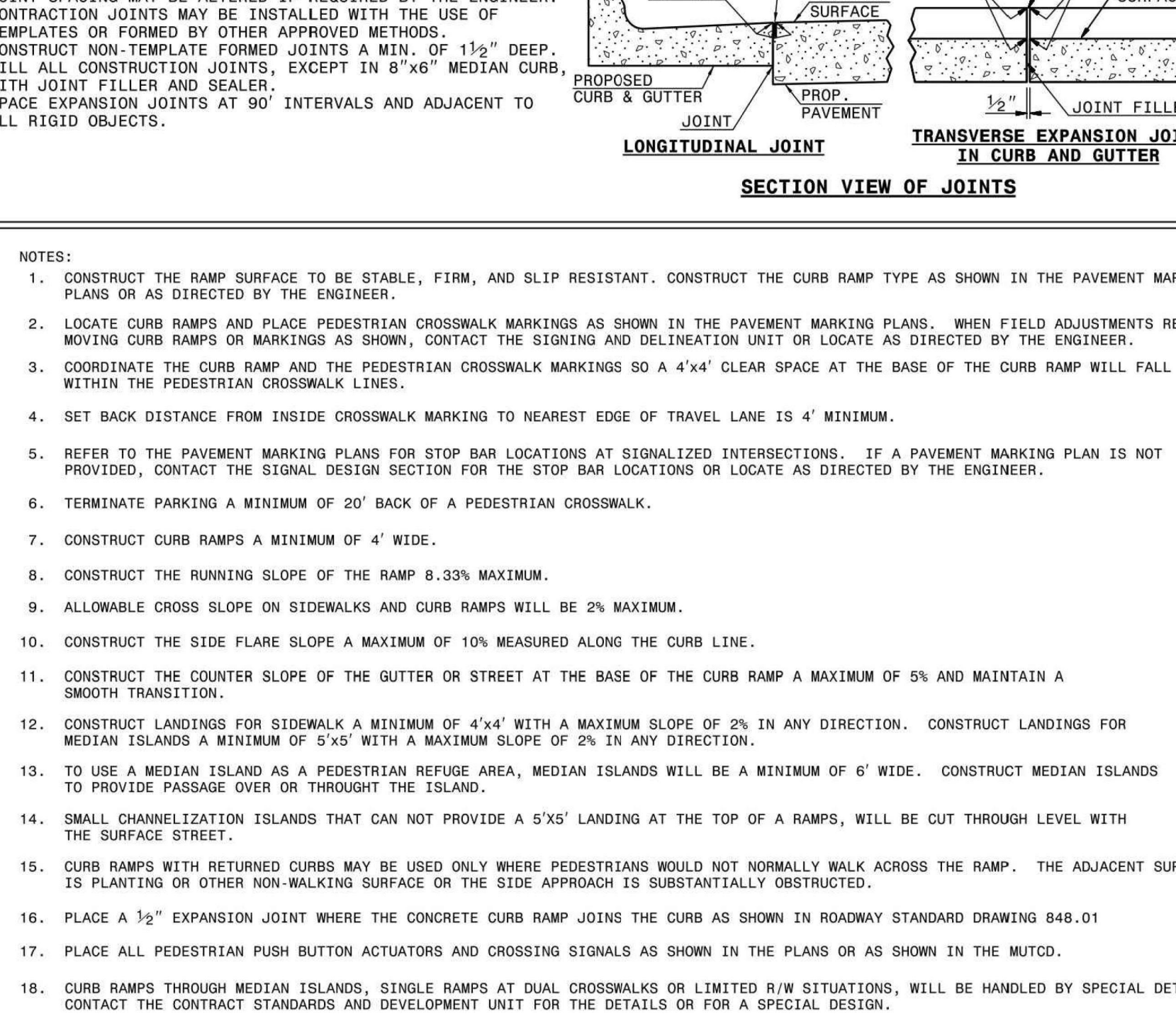
01 SAND VOLLEYBALL COURT
SCALE: 1/8"=1'-0"



BICYCLE RACK
N.T.S.



ACCESSIBLE SIDEWALK RAMP
N.T.S.



SECTION VIEW OF JOINTS

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAY STANDARD DRAWING FOR
CURB RAMP AND GUTTER

NOTES:

- CONSTRUCT THE RAMP SURFACE TO BE STABLE, FIRM, AND SLIP RESISTANT. CONSTRUCT THE CURB RAMP TYPE AS SHOWN IN THE PAVEMENT MARKING PLANS OR AS DIRECTED BY THE ENGINEER.
- LOCATE CURB RAMP AND PLACE PEDESTRIAN CROSSWALK MARKINGS AS SHOWN IN THE PAVEMENT MARKING PLANS. WHEN FIELD ADJUSTMENTS REQUIRE MOVING CURB RAMP OR MARKINGS AS SHOWN, CONTACT THE SIGNING AND DELINEATION UNIT OR LOCATE AS DIRECTED BY THE ENGINEER.
- COORDINATE THE CURB RAMP AND THE PEDESTRIAN CROSSWALK MARKINGS SO A 4'x4' CLEAR SPACE AT THE BASE OF THE CURB RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES.
- SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL LANE IS 4' MINIMUM.
- REFER TO THE PAVEMENT MARKING PLANS FOR STOP BAR LOCATIONS AT SIGNALIZED INTERSECTIONS. IF A PAVEMENT MARKING PLAN IS NOT PROVIDED, CONTACT THE SIGNAL DESIGN SECTION FOR THE STOP BAR LOCATIONS OR LOCATE AS DIRECTED BY THE ENGINEER.
- TERMINATE PARKING A MINIMUM OF 20' BACK OF A PEDESTRIAN CROSSWALK.
- CONSTRUCT CURB RAMP A MINIMUM OF 4' WIDE.
- CONSTRUCT THE RUNNING SLOPE OF THE RAMP 0.33% MAXIMUM.
- ALLOWABLE CROSS SLOPE ON SIDEWALKS AND CURB RAMP WILL BE 2% MAXIMUM.
- CONSTRUCT THE SIDE FLARE SLOPE A MAXIMUM OF 10% MEASURED ALONG THE CURB LINE.
- CONSTRUCT THE COUNTER SLOPE OF THE GUTTER OR STREET AT THE BASE OF THE CURB RAMP A MAXIMUM OF 5% AND MAINTAIN A SMOOTH TRANSITION.
- CONSTRUCT LANDINGS FOR SIDEWALK A MINIMUM OF 4'x4' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION. CONSTRUCT LANDINGS FOR MEDIAN ISLANDS A MINIMUM OF 5'x5' WITH A MAXIMUM SLOPE OF 2% IN ANY DIRECTION.
- TO USE A MEDIAN ISLAND AS A PEDESTRIAN REFUGE AREA, MEDIAN ISLANDS WILL BE A MINIMUM OF 6' WIDE. CONSTRUCT MEDIAN ISLANDS TO PROVIDE PASSAGE OVER OR THROUGH THE ISLAND.
- SMALL CHANNELIZATION ISLANDS THAT CAN NOT PROVIDE A 5'x5' LANDING AT THE TOP OF A RAMP, WILL BE CUT THROUGH LEVEL WITH THE SURFACE STREET.
- CURB RAMP WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP. THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED.
- PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE CURB RAMP JOINS THE CURB AS SHOWN IN ROADWAY STANDARD DRAWING 848.01
- PLACE ALL PEDESTRIAN PUSH BUTTON ACTUATORS AND CROSSING SIGNALS AS SHOWN IN THE PLANS OR AS SHOWN IN THE MUTCD.
- CURB RAMP THROUGH MEDIAN ISLANDS, SINGLE RAMP AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE HANDLED BY SPECIAL DETAILS. CONTACT THE CONTRACT STANDARDS AND DEVELOPMENT UNIT FOR THE DETAILS OR FOR A SPECIAL DESIGN.

SHEET 3 OF 3
848.05

McADAMS
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Durham, NC 27713

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fax 919.361.2269
license number: C-0293, C-187

www.mcadamsco.com

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ASHTON WOODS RESIDENTIAL, LLC.
900 RIDGEFIELD DRIVE, SUITE 335
RALEIGH, NORTH CAROLINA 27609
PHONE: 919.422.7663
CONTACT: BOB MISHLER

ASHTON WOODS

THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08

REVISIONS

NO.	DATE

PLAN INFORMATION

PROJECT NO. AWH-20000
FILENAME AWH20000-CD-PKG-02-01
CHECKED BY .
DRAWN BY .
SCALE N.T.S.
DATE 05.08.2023

SHEET

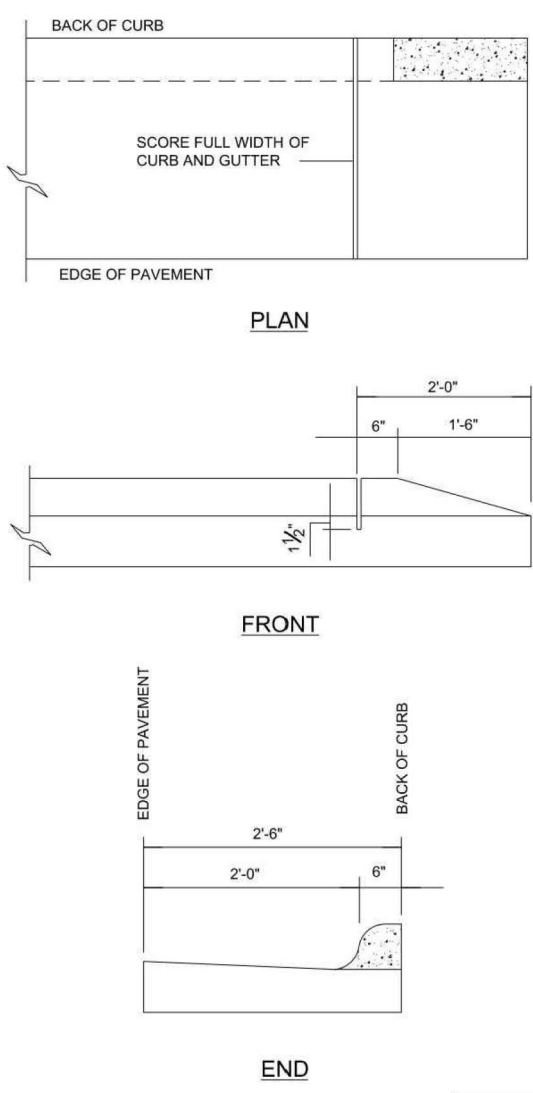
SITE DETAILS

C8.00

SEAL
WILLIAM T. O'DANIEL
ENGINEER
NORTH CAROLINA PROFESSIONAL SEAL 22630
William T O'Daniel
c/o=William T O'Daniel, c=US,
o=North Carolina
email=odaniel@mcadamsco.com
2023.05.05 15:26:03 -04'00'

FINAL DRAWING - RELEASED FOR CONSTRUCTION

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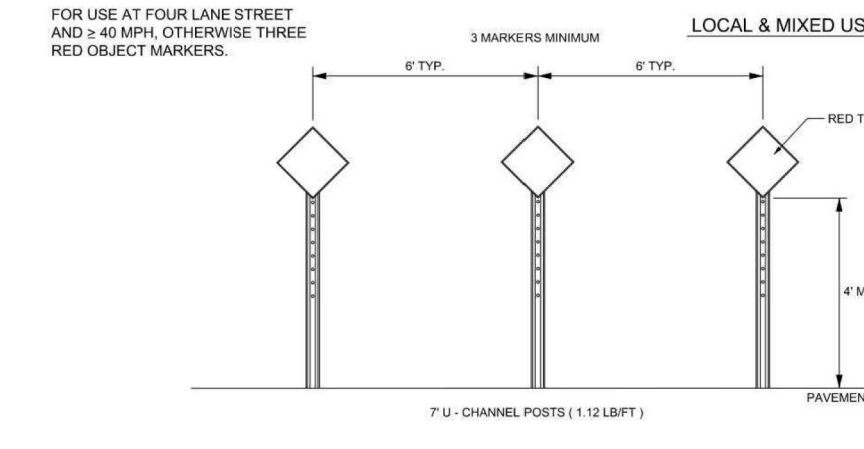
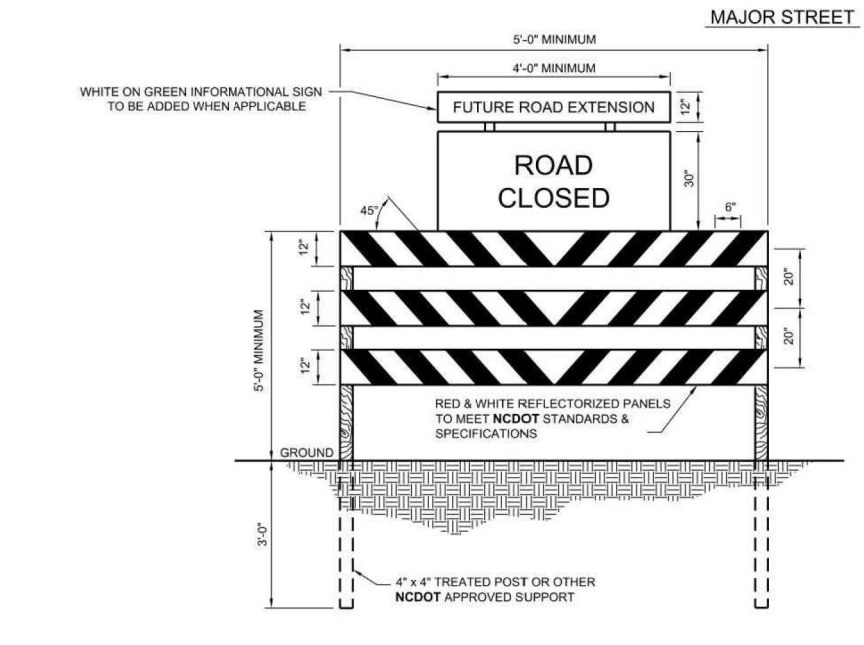


CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

STANDARD METHOD OF ENDING CURB AND GUTTER

T-10.25

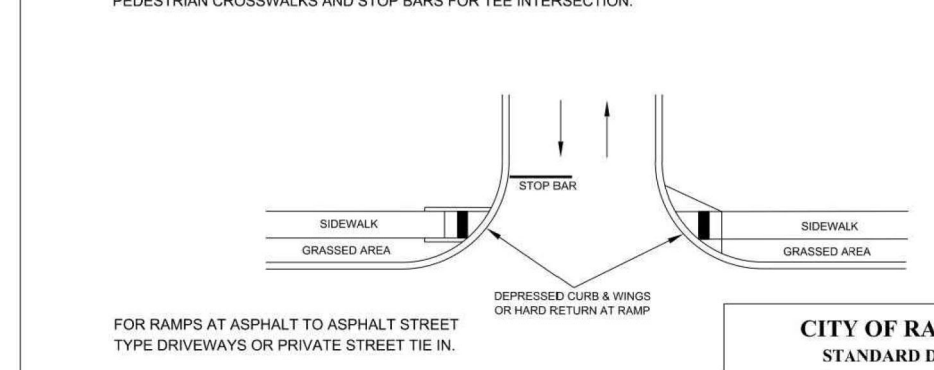
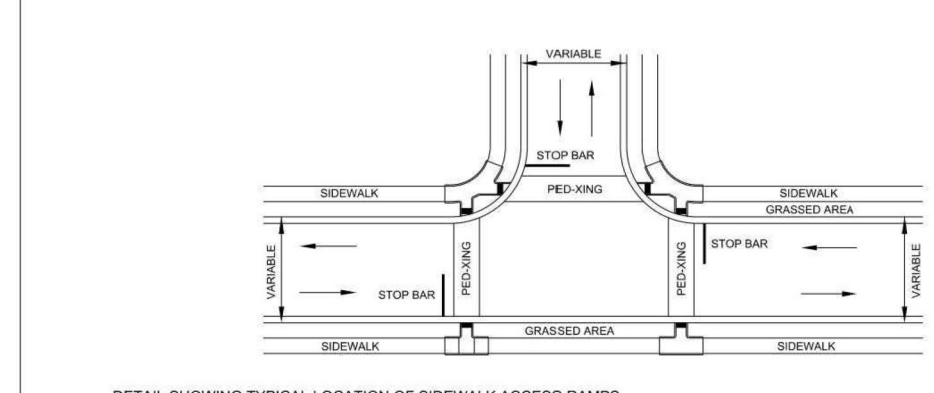
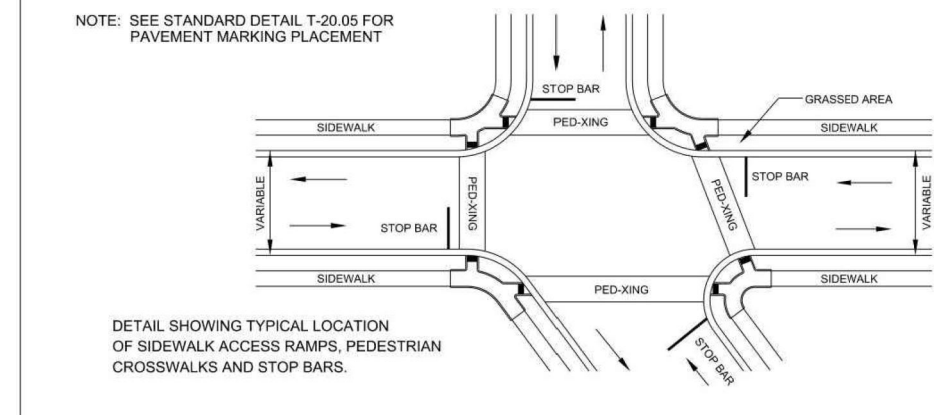


CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

TEMPORARY BARRICADE FOR DEAD END ROADS

T-20.01.28

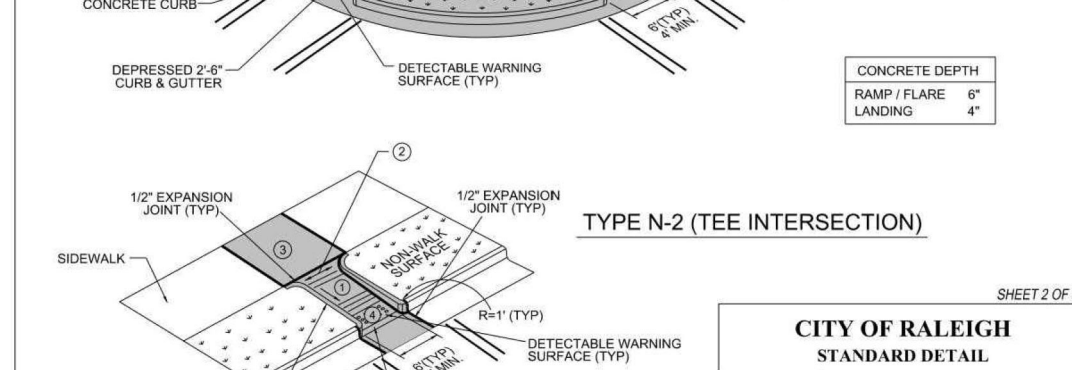
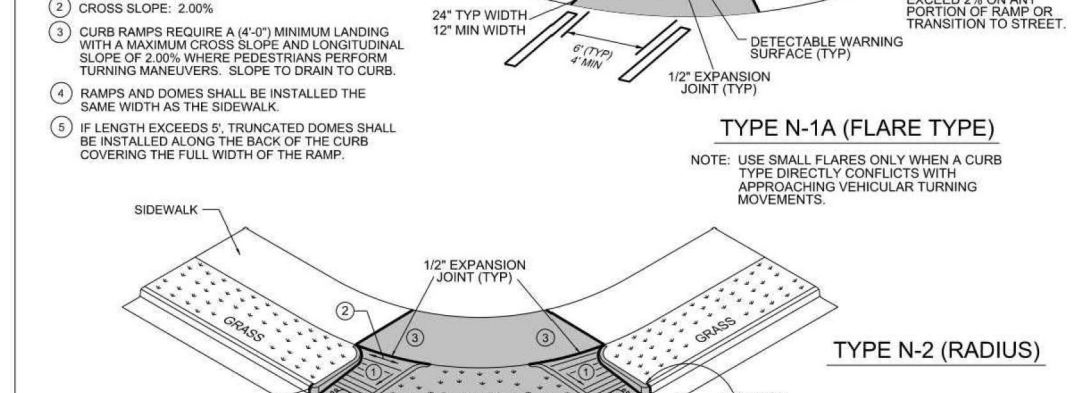
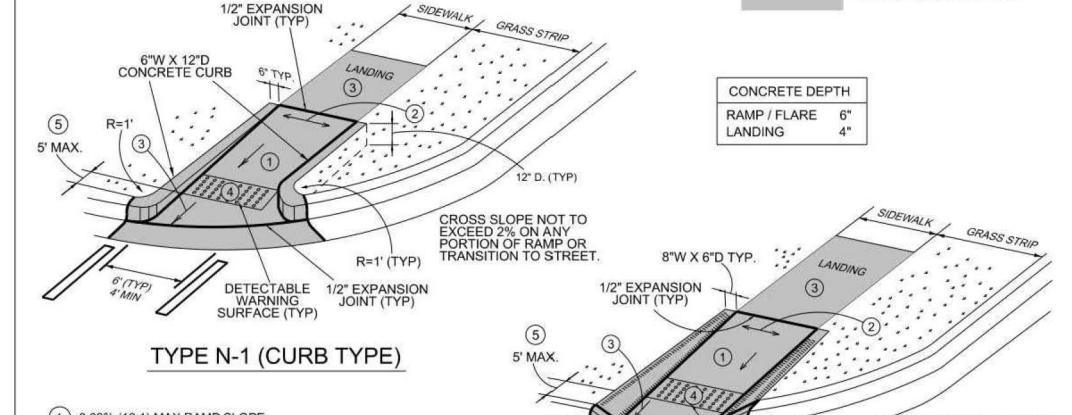


CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

CURB RAMPS (NEW DEVELOPMENT)

T-20.01.1

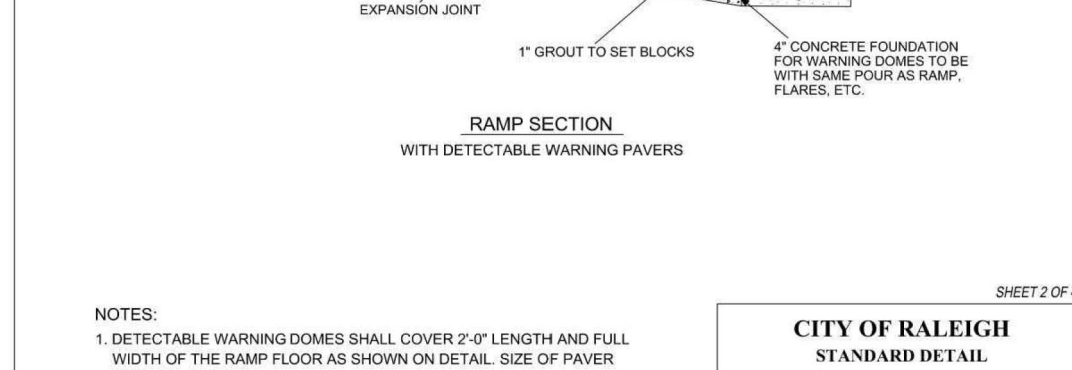
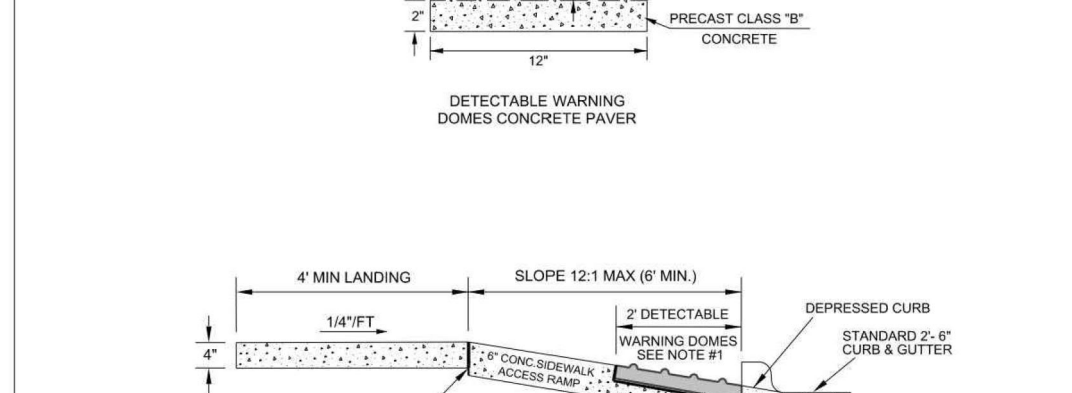
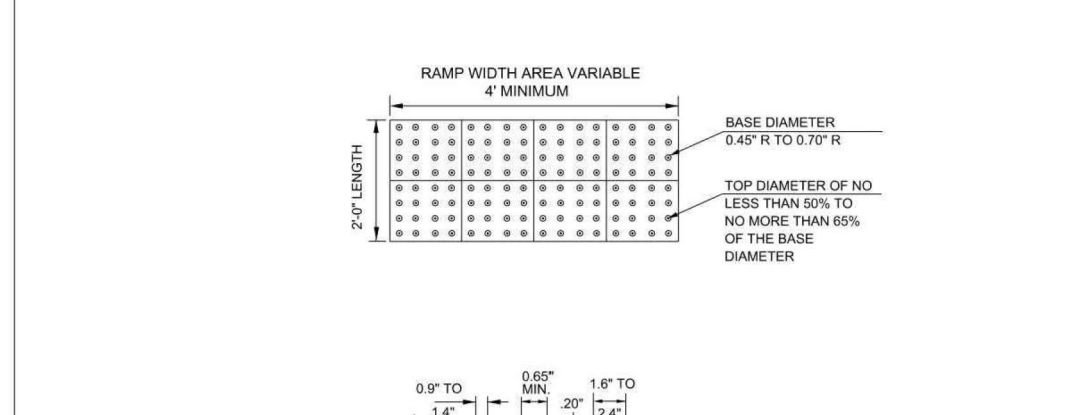


CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

CURB RAMPS (NEW DEVELOPMENT)

T-20.01.2

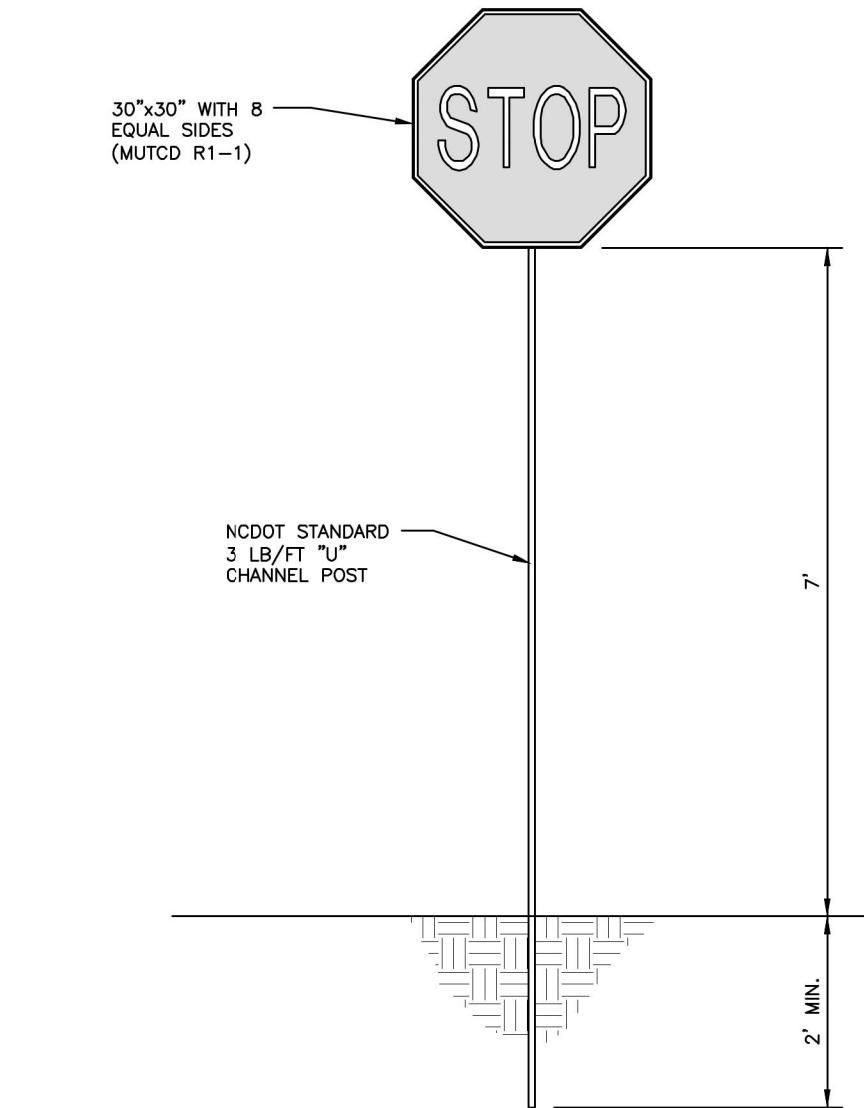


CITY OF RALEIGH
STANDARD DETAIL

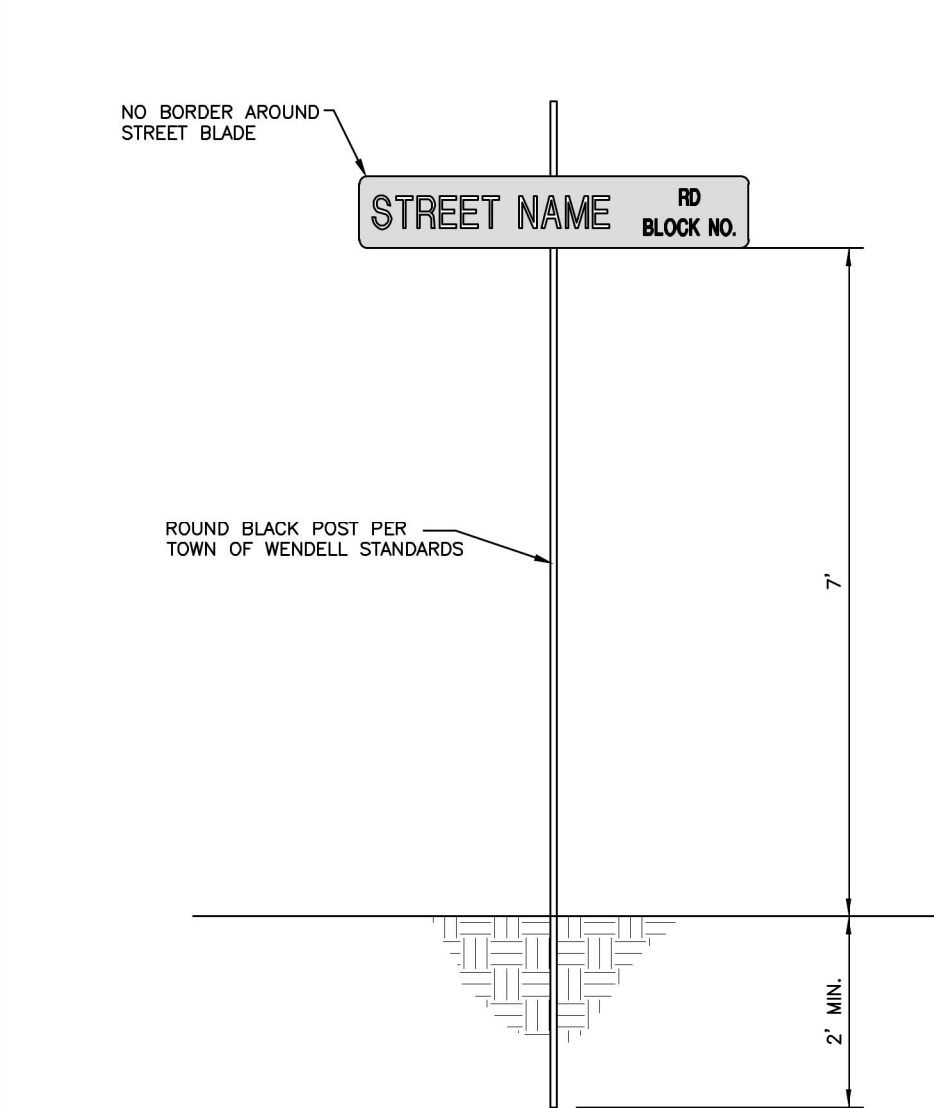
REVISIONS	DATE	BY	NOT TO SCALE

DETECTABLE WARNING SURFACE PAVERS

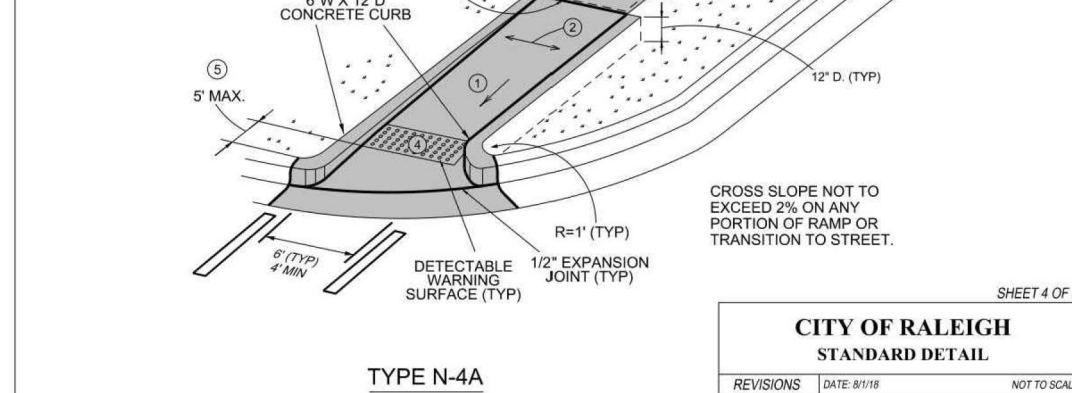
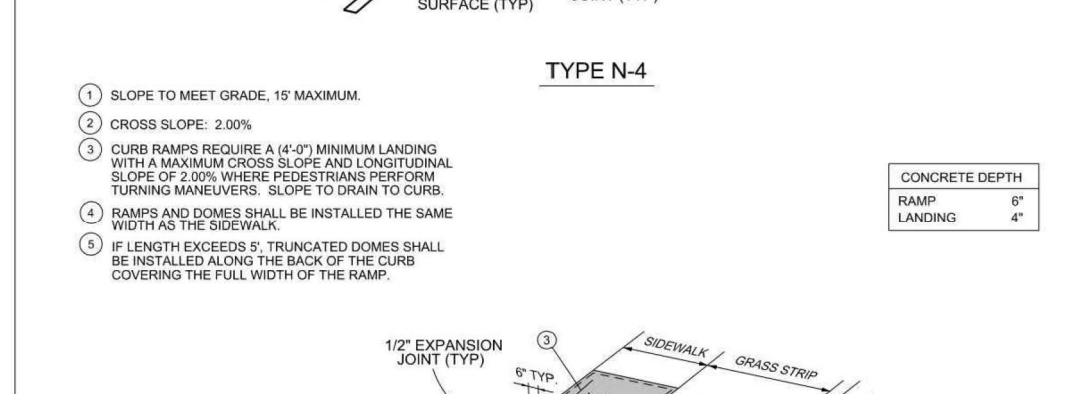
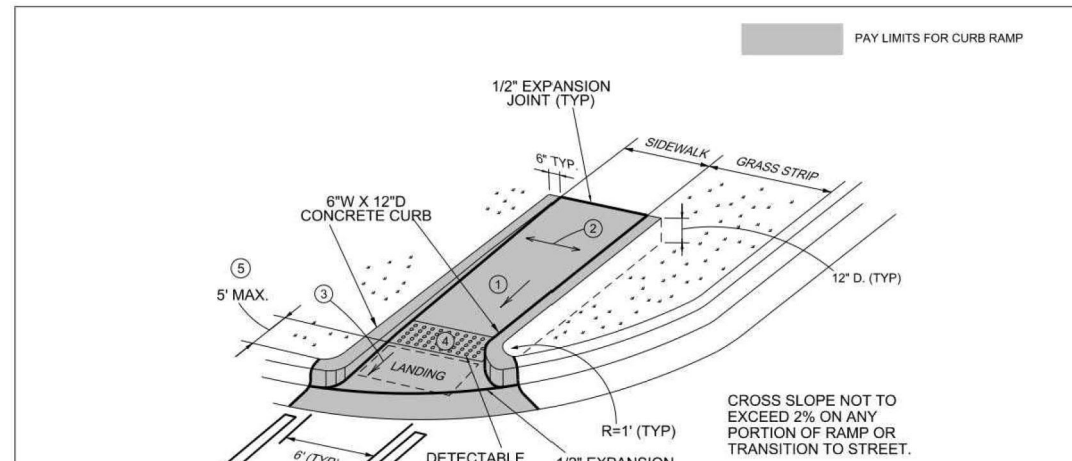
T-20.04.2



STOP SIGN DETAIL
N.T.S.



STREET SIGN DETAIL
N.T.S.

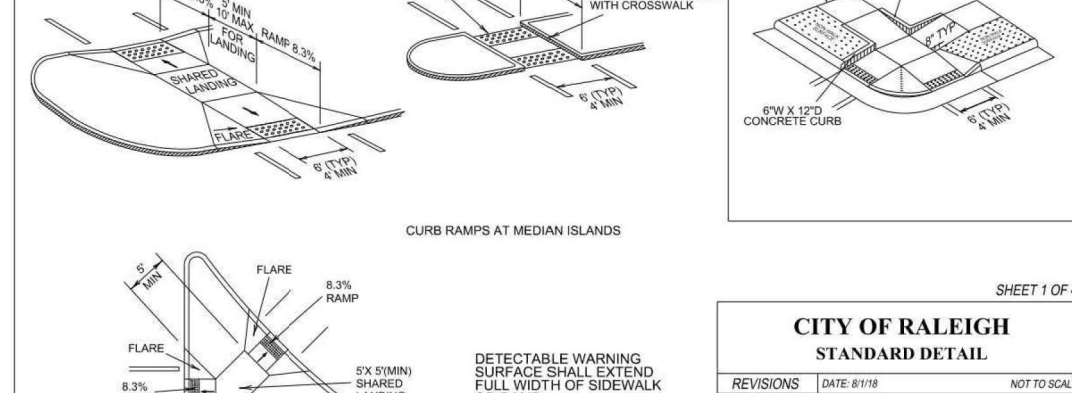
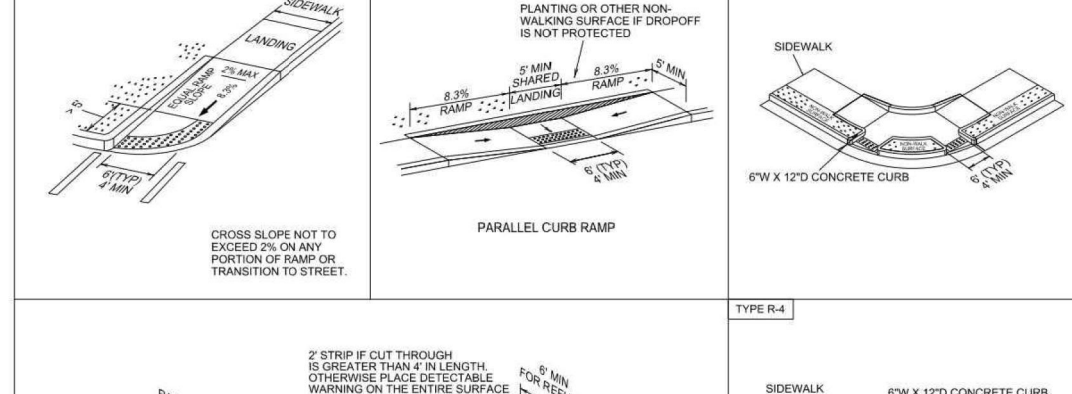
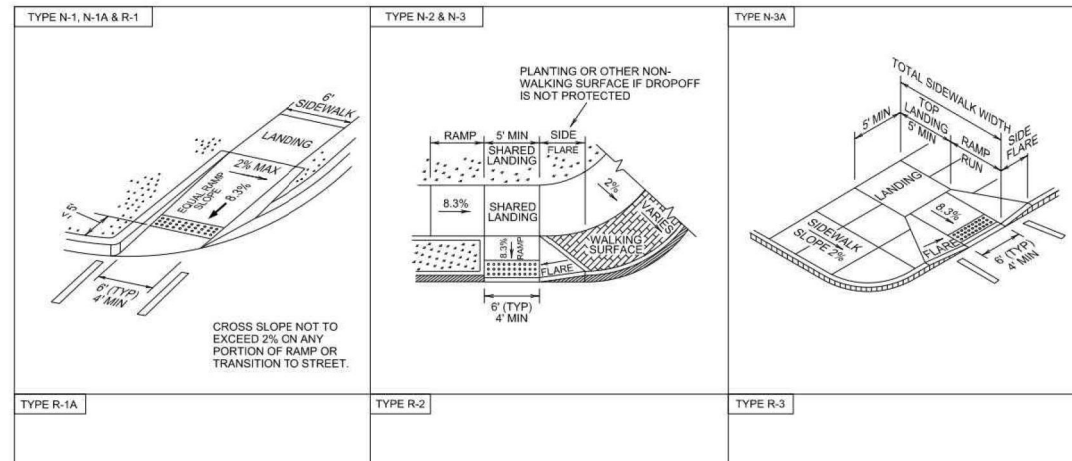


CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

CURB RAMPS (NEW DEVELOPMENT)

T-20.01.4

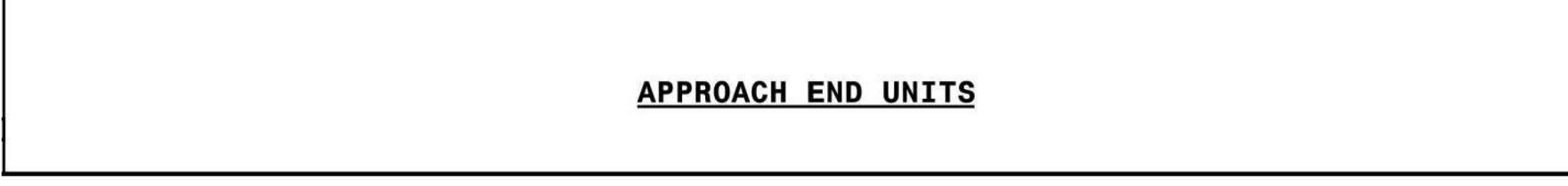
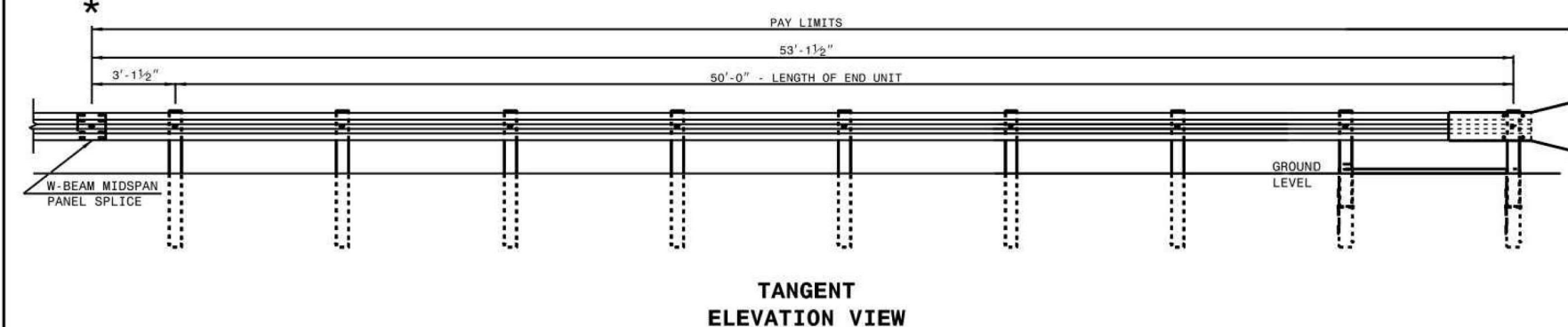
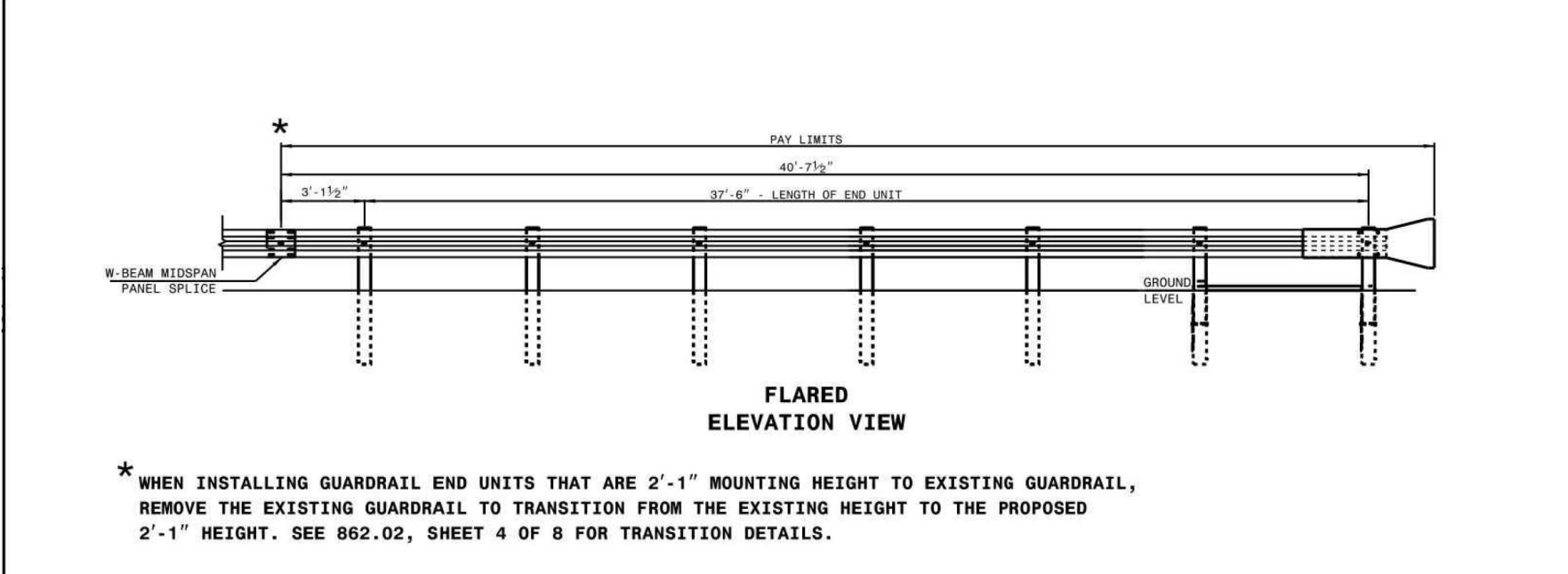


CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

DETECTABLE WARNING SURFACE PLACEMENT

T-20.04.1

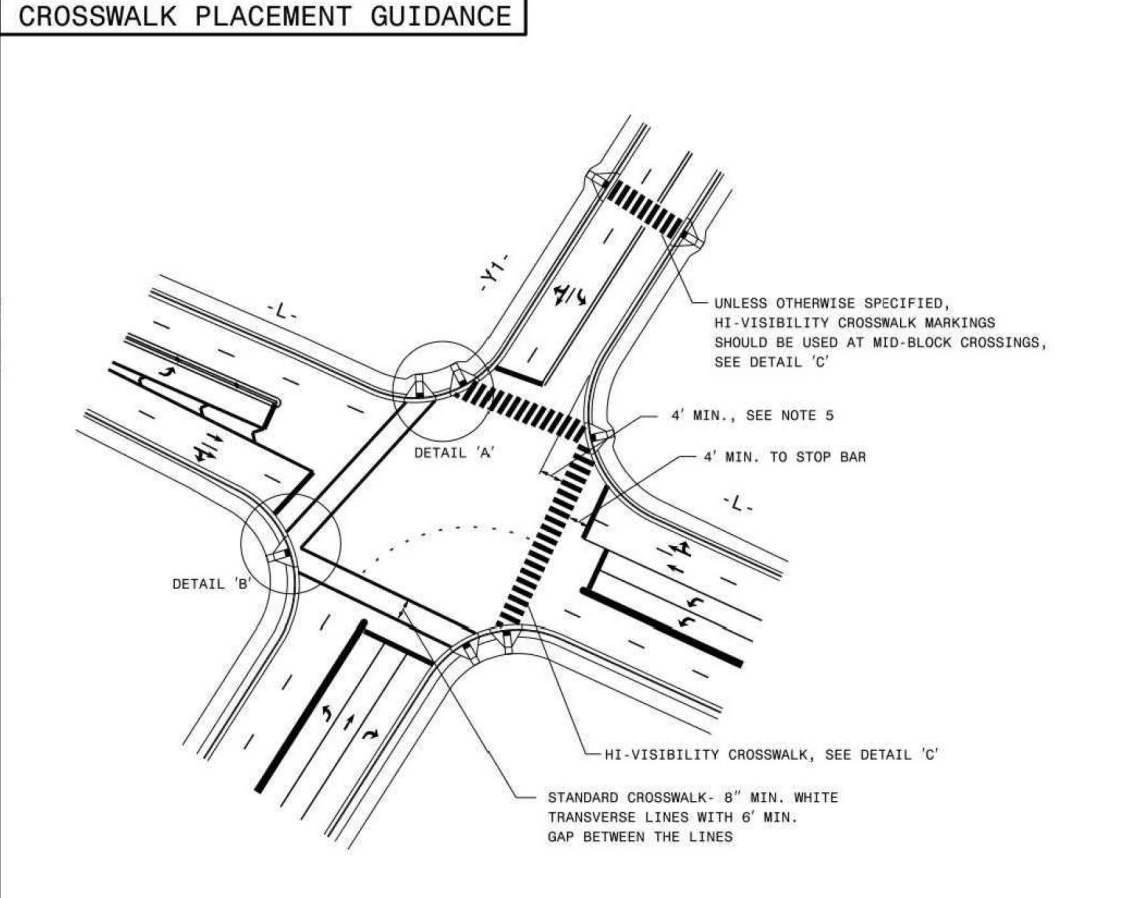


FLARED ELEVATION VIEW
TANGENT ELEVATION VIEW
APPROACH END UNITS

STATE OF NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ROADWAY STANDARD DRAWING FOR
GUARDRAIL INSTALLATION

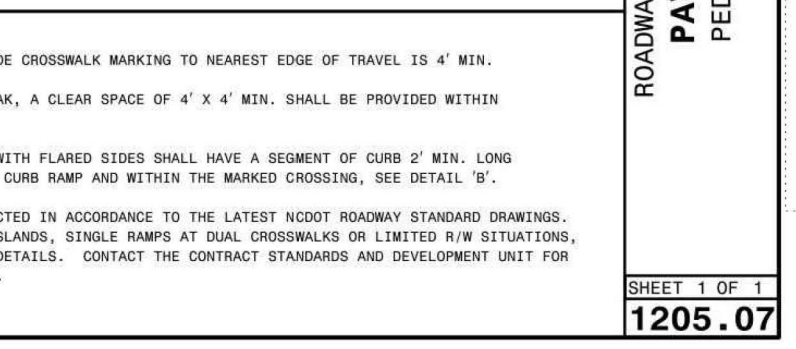
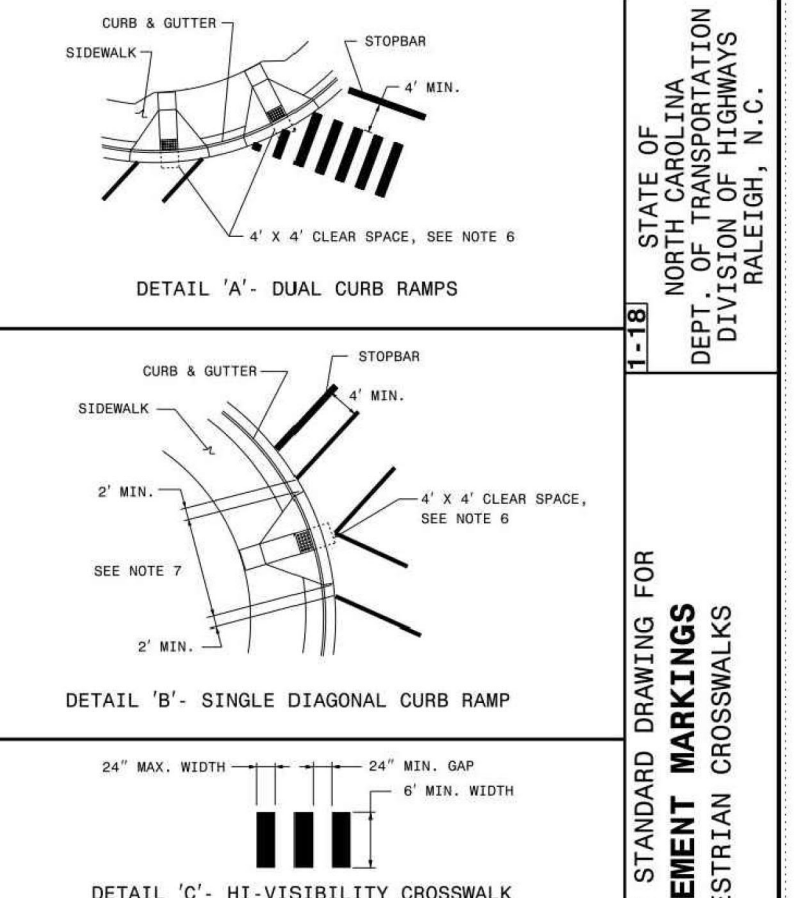
SHEET 2 OF 8
862.02



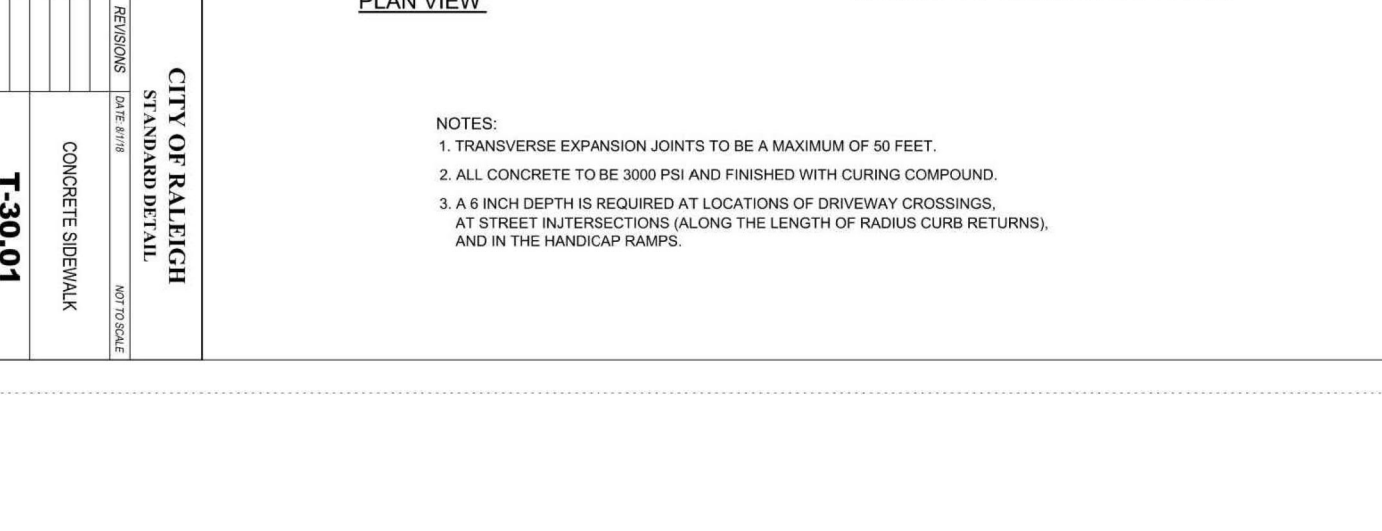
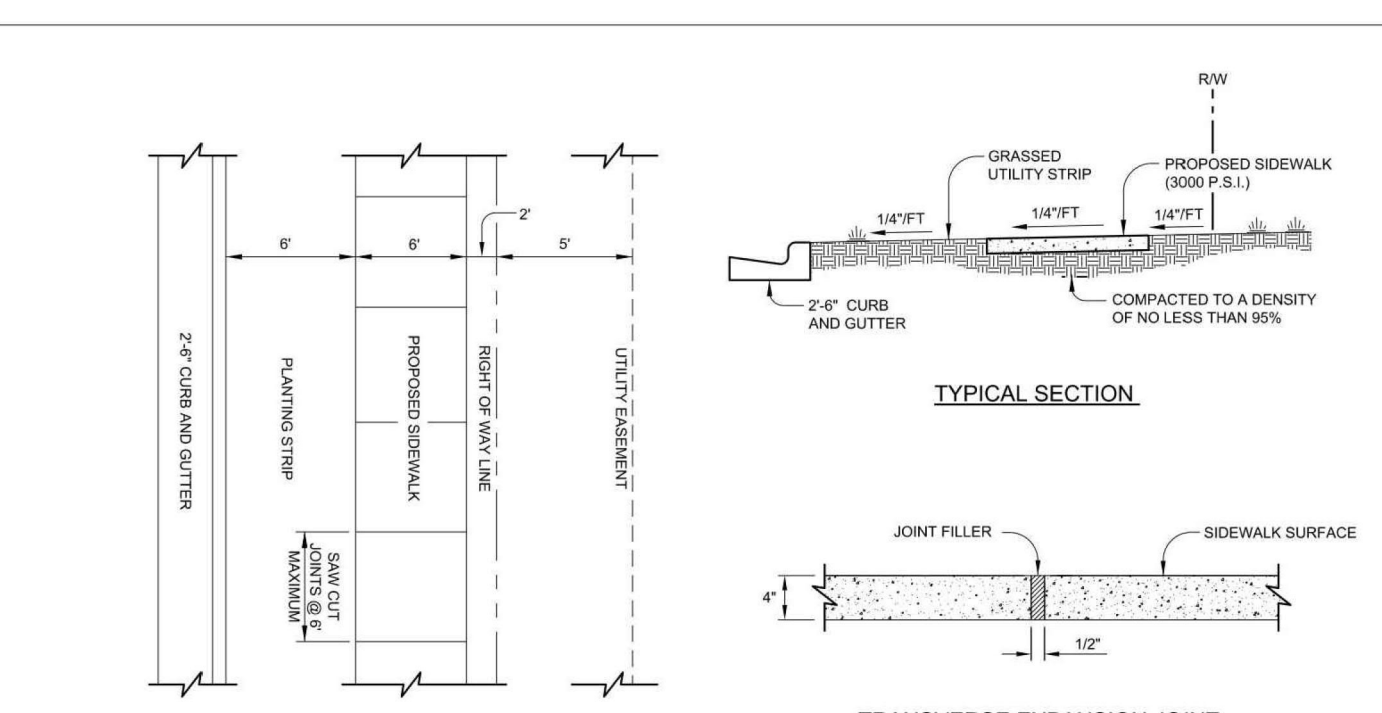
CROSSWALK PLACEMENT GUIDANCE

GENERAL NOTES:

- USE THE DETAILS ABOVE AND THE FOLLOWING NOTES FOR GUIDANCE IN PLACING CROSSWALK MARKINGS. REFER TO NCDOT ROADWAY STANDARD DRAWINGS, MUTCD AND ADA STANDARDS FOR ADDITIONAL GUIDANCE.
- THE LOCATION AND TYPE OF CROSSWALK MARKINGS SHOWN ON THE ABOVE DETAILS ARE FOR REFERENCE ONLY. LOCATE CROSSWALK MARKINGS AS SHOWN ON THE PROJECT DETAIL SHEETS OR AS DIRECTED BY THE ENGINEER. THE CROSSWALK MARKING TYPE, STANDARD OR HI-VISIBILITY, SHOULD BE INSTALLED AS SPECIFIED ON THE PROJECT DETAIL SHEETS OR AS DIRECTED BY THE ENGINEER.
- THE STANDARD CROSSWALK IS TWO WHITE 8" MIN. TRANSVERSE LINES WITH A 4" MIN. GAP BETWEEN THE LINES. THE HI-VISIBILITY CROSSWALK IS WHITE 24" MAX. WIDE LONGITUDINAL LINES WITH 24" MIN. GAPS BETWEEN LINES. SEE DETAIL 'C'. HI-VISIBILITY CROSSWALKS SHOULD BE A MINIMUM OF 8' WIDE. CURB RAMPS SHALL BE FULLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED.
- STOP BARS SHOULD BE PLACED A 4' MIN. IN ADVANCE OF NEAREST CROSSWALK LINE.
- SET BACK DISTANCE FROM INSIDE CROSSWALK MARKING TO NEAREST EDGE OF TRAVEL IS 4' MIN.
- BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE OF 4' X 4' MIN. SHALL BE PROVIDED WITHIN THE MARKINGS.
- SINGLE DIAGONAL CURB RAMPS WITH FLARED SIDES SHALL HAVE A SEGMENT OF CURB 2' MIN. LONG LOCATED ON EACH SIDE OF THE CURB RAMP AND WITHIN THE MARKED CROSSING. SEE DETAIL 'B'.
- CURB RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE TO THE LATEST NCDOT ROADWAY STANDARD DRAWINGS. CURB RAMPS THROUGH MEDIAN ISLANDS, SINGLE RAMPS AT DUAL CROSSWALKS OR LIMITED R/W SITUATIONS, WILL BE SHOWN BY SPECIAL DETAILS. CONTACT THE CONTRACT STANDARDS AND DEVELOPMENT UNIT FOR DETAILS ON A SPECIAL DESIGN.



DETAIL 'A' - DUAL CURB RAMPS
DETAIL 'B' - SINGLE DIAGONAL CURB RAMP
DETAIL 'C' - HI-VISIBILITY CROSSWALK



TYPICAL SECTION
TRANSVERSE EXPANSION JOINT

CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

DETECTABLE WARNING SURFACE PLACEMENT

T-20.01

McADAMS
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fax 919. 361. 2269
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CLIENT

ASHTON WOODS RESIDENTIAL, LLC.
900 RIDGEFIELD DRIVE, SUITE 335
RALEIGH, NORTH CAROLINA 27609
PHONE: 919. 422. 7663
CONTACT: BOB MISHLER

ASHTON WOODS

THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08

CITY OF RALEIGH
STANDARD DETAIL

REVISIONS	DATE	BY	NOT TO SCALE

DETECTABLE WARNING SURFACE PLACEMENT

T-20.04.1

William T. O'Daniel
Professional Engineer
Seal 22630
2023.05.05 15:26:18 -04'00'

REVISIONS

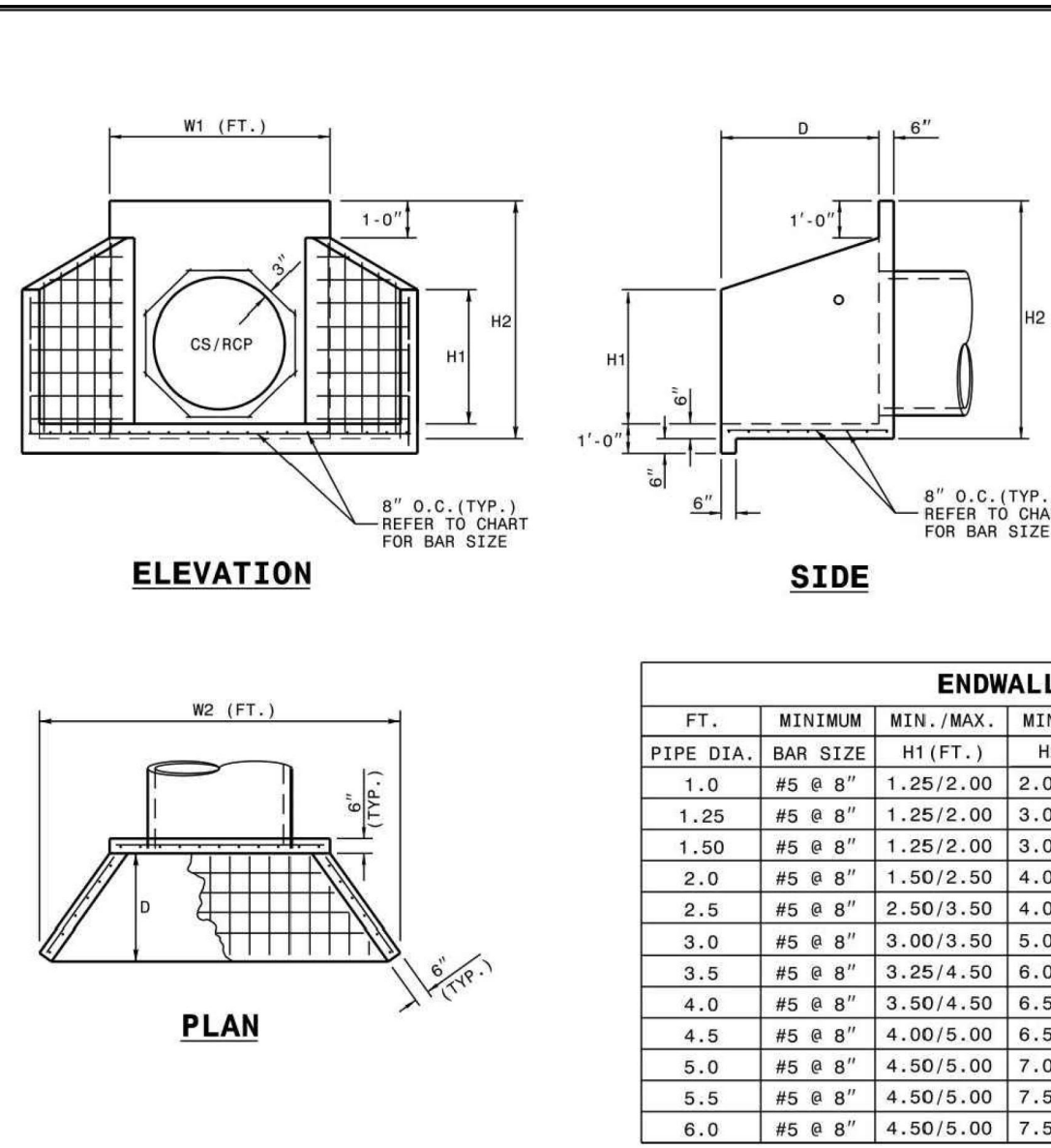
NO.	DATE

PLAN INFORMATION

PROJECT NO. AWH-20000
FILENAME AWH20000-CD-PKG-02-D1
CHECKED BY
DRAWN BY
SCALE N.T.S.
DATE 05.08.2023

SHEET

C8.01

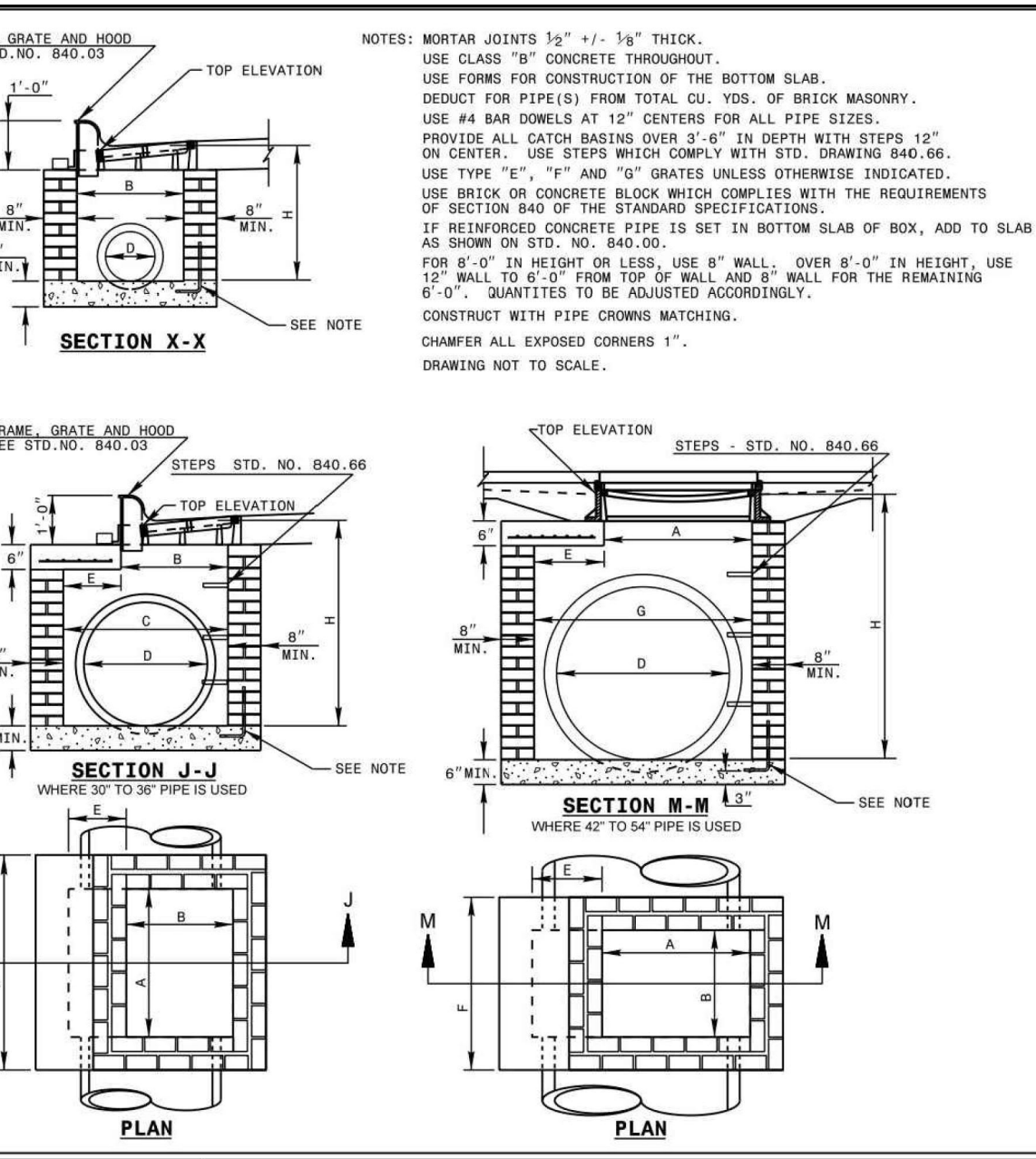
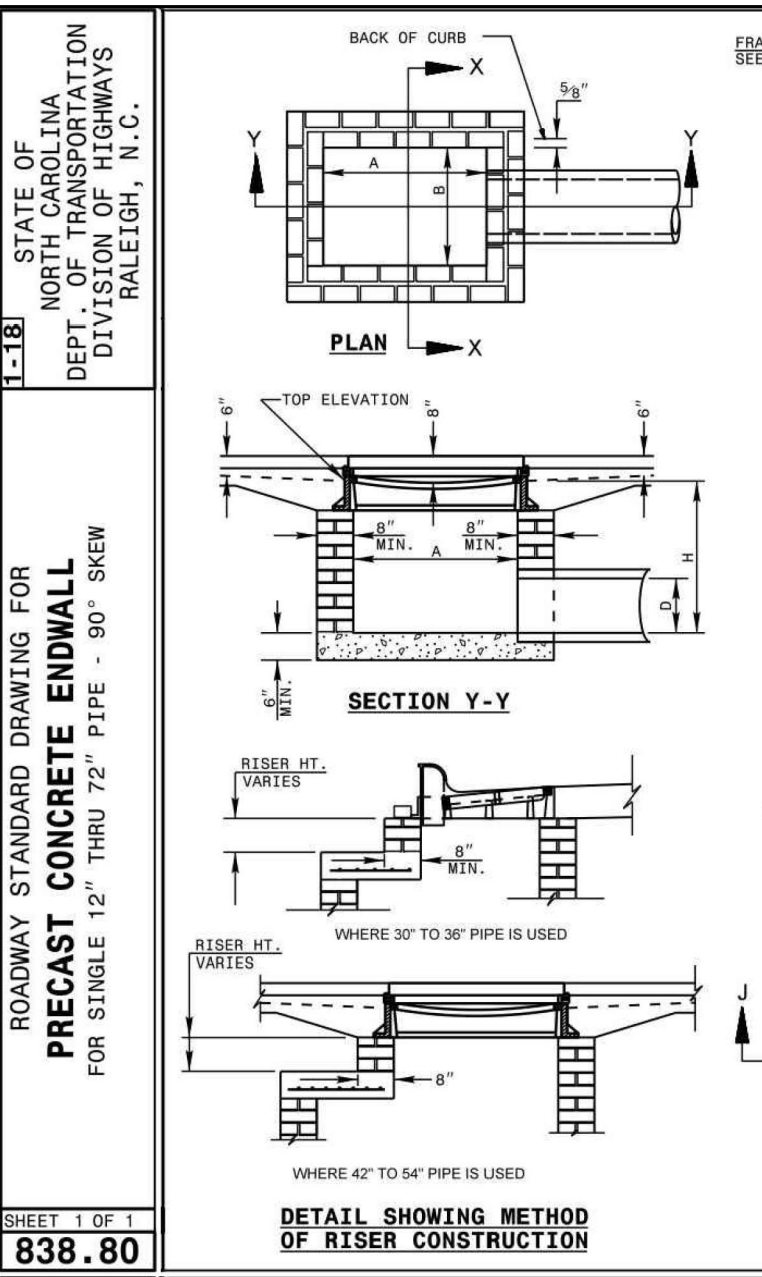


NOTES:
 * THIS PRECAST ENDWALL MAY BE USED FOR THE FOLLOWING STANDARDS: 838.01, 838.11, 838.21, 838.27, 838.33, 838.39, 838.51, 838.57, 838.63 AND 838.69.
 * INSTALL PRECAST ENDWALLS WITH WINGS AND PAY FOR IN ACCORDANCE WITH SPECIFICATION SECTION 838.
 * USE 4000 PSI CONCRETE.
 * PROVIDE ALL REINFORCING STEEL WHICH MEETS ASTM A615 FOR GRADE 60 AND WELDED WIRE FABRIC CONFORMING TO ASTM A185 WITH 2" MIN. CLEARANCE.
 * PLACE LIFT HOLES OR PINS IN ACCORDANCE WITH SPECIFICATION SECTION 1008.704.
 * PIPE TO BE GROUNDED INTO HEADWALL AT JOB SITE BY CONTRACTOR.
 * ALL ELEMENTS PRECAST TO MEET ASTM C913.
 * WELDED WIRE FABRIC MAY BE SUBSTITUTED FOR REBAR AS LONG AS THE SAME AREA OF STEEL IS PROVIDED.
 * CHAMFER ALL EXPOSED CORNERS 1" OR HAVE A RADIUS OF 1".

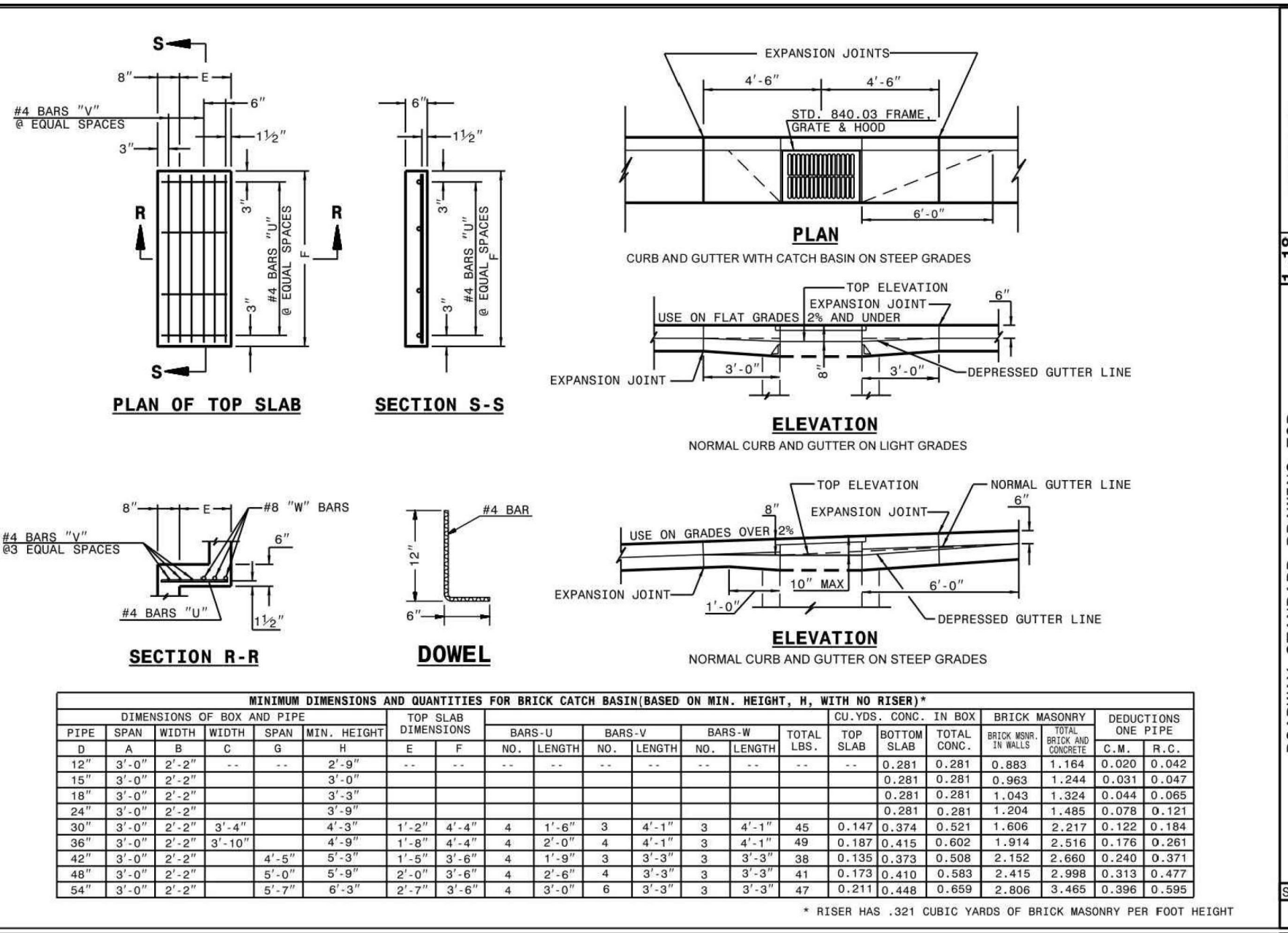
NOTE: THE MINIMUM BAR SIZE SHALL BE #5 BARS AT 8" CTS. THE CONTRACTOR WILL HAVE THE OPTION TO INCREASE THIS BAR SIZE AS NEEDED.

ENDWALL DIMENSIONS							
PIPE DIA.	MINIMUM BAR SIZE	MIN./MAX. H1 (FT.)	MIN./MAX. H2 (FT.)	MIN./MAX. D (FT.)	W1	W2	MIN./MAX. W2
1.0	#5 @ 8"	1.25/2.00	2.00/3.75	1.25/1.75	3.00/3.75	5.50/6.00	
1.25	#5 @ 8"	1.25/2.00	3.00/3.75	1.25/2.00	3.50/3.75	6.50/6.75	
1.50	#5 @ 8"	1.25/2.00	3.00/4.25	1.50/2.50	3.50/3.75	6.50/6.75	
2.0	#5 @ 8"	1.50/2.50	4.00/4.75	1.75/2.50	4.00/4.25	7.50/8.25	
2.5	#5 @ 8"	2.50/3.50	4.00/6.00	2.00/3.00	4.50/5.50	10.00/11.50	
3.0	#5 @ 8"	3.00/3.50	5.00/6.00	2.75/3.50	5.25/5.75	11.50/11.75	
3.5	#5 @ 8"	3.25/4.50	6.00/6.75	3.25/3.50	6.00/6.75	12.00/13.25	
4.0	#5 @ 8"	3.50/4.50	6.50/7.00	3.25/3.50	6.50/6.75	13.00/13.25	
4.5	#5 @ 8"	4.00/5.00	6.50/8.50	3.25/4.00	7.00/9.25	13.50/15.75	
5.0	#5 @ 8"	4.50/5.00	7.00/8.50	3.25/4.00	7.25/9.25	13.75/15.75	
5.5	#5 @ 8"	4.50/5.00	7.50/8.50	3.25/4.00	7.25/9.25	14.00/15.75	
6.0	#5 @ 8"	4.50/5.00	7.50/8.50	3.25/4.00	7.75/9.25	14.75/16.75	

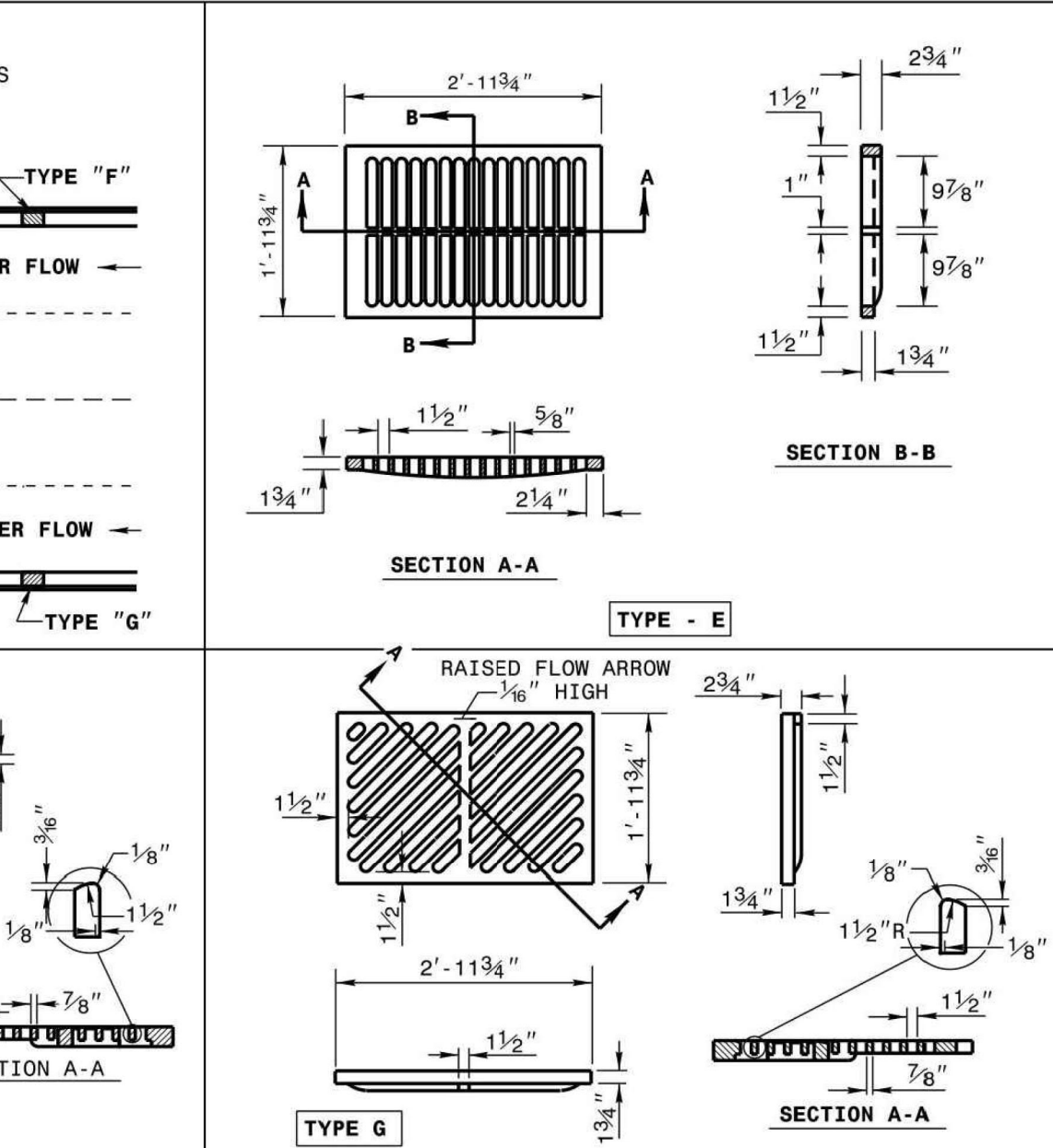
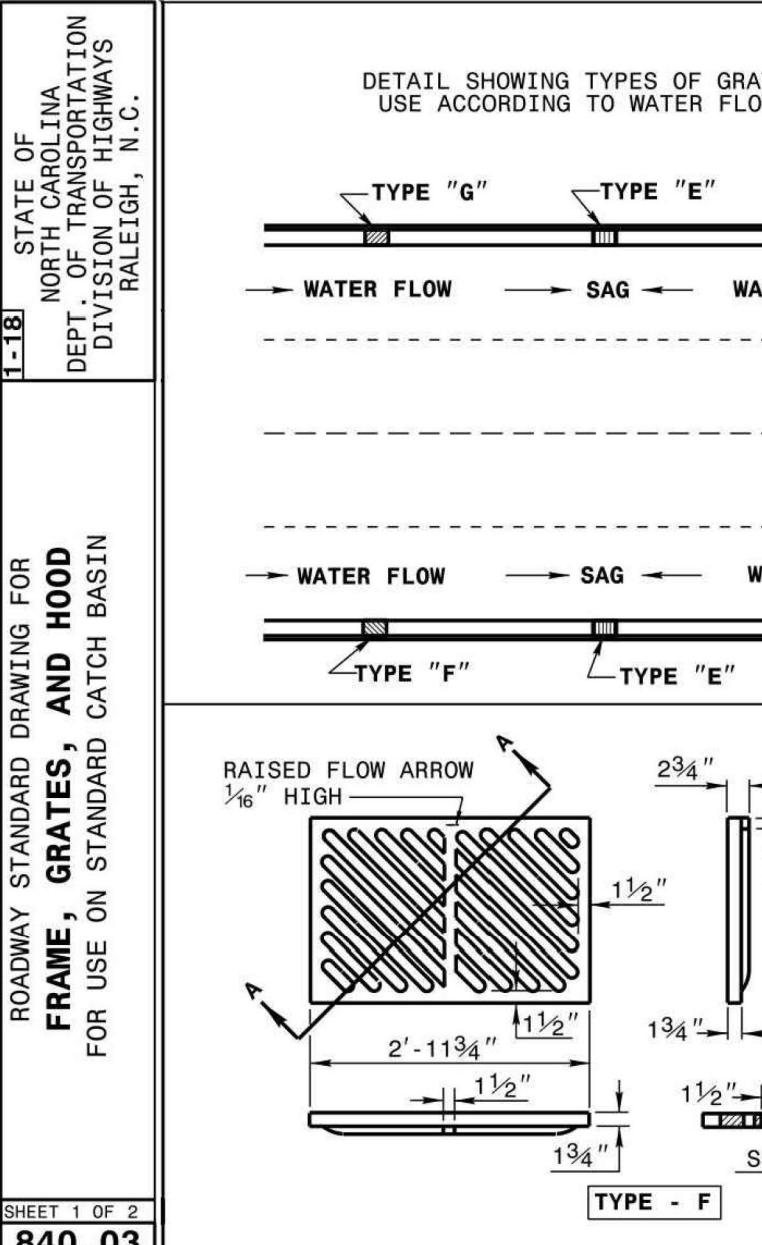
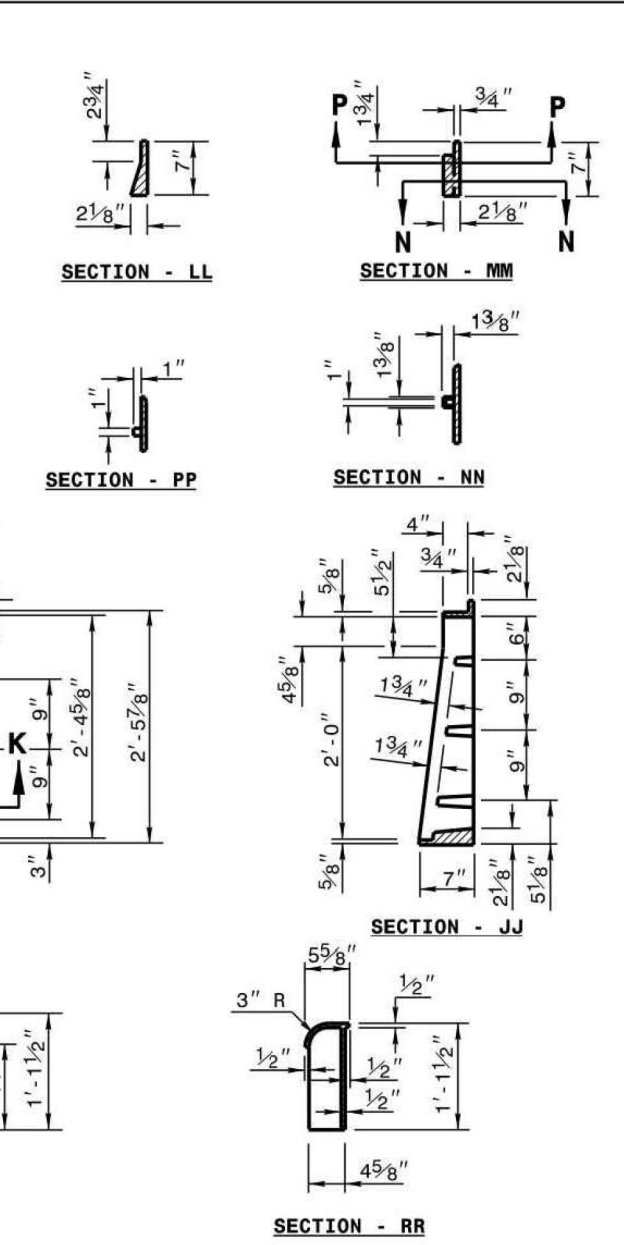
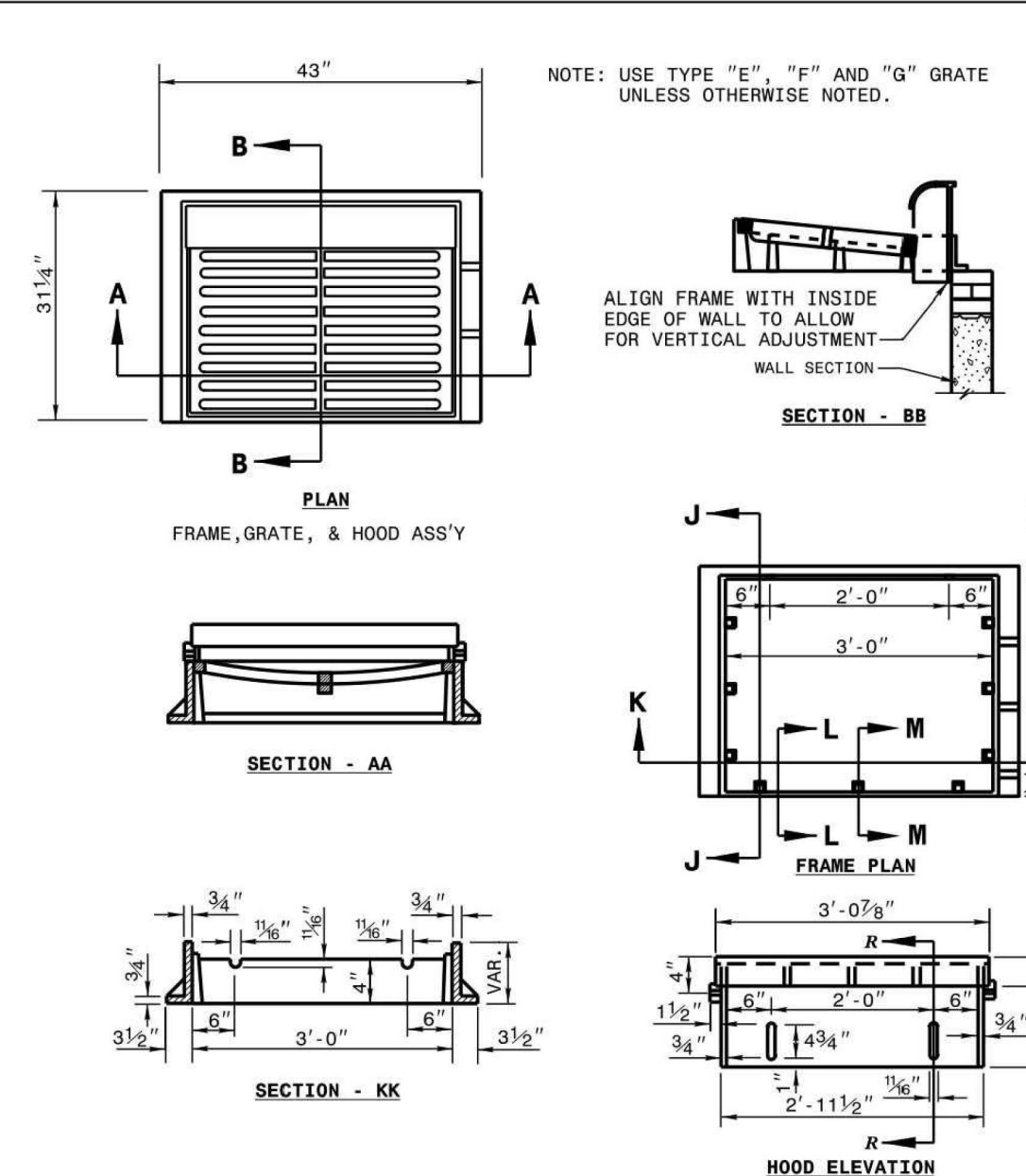
STATE OF NORTH CAROLINA DEPT. OF TRANSPORTATION DIVISION OF HIGHWAYS RALEIGH, N.C.



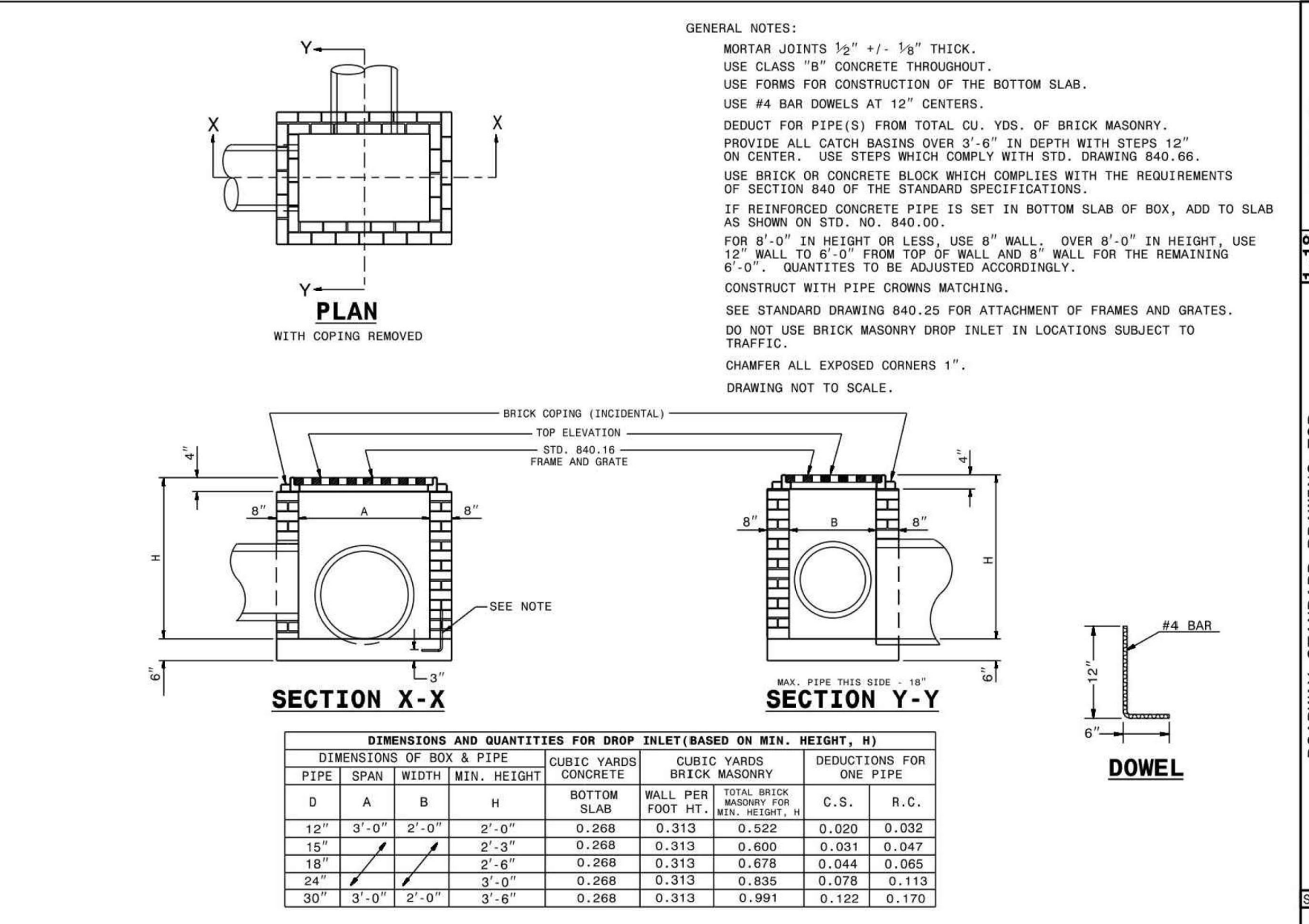
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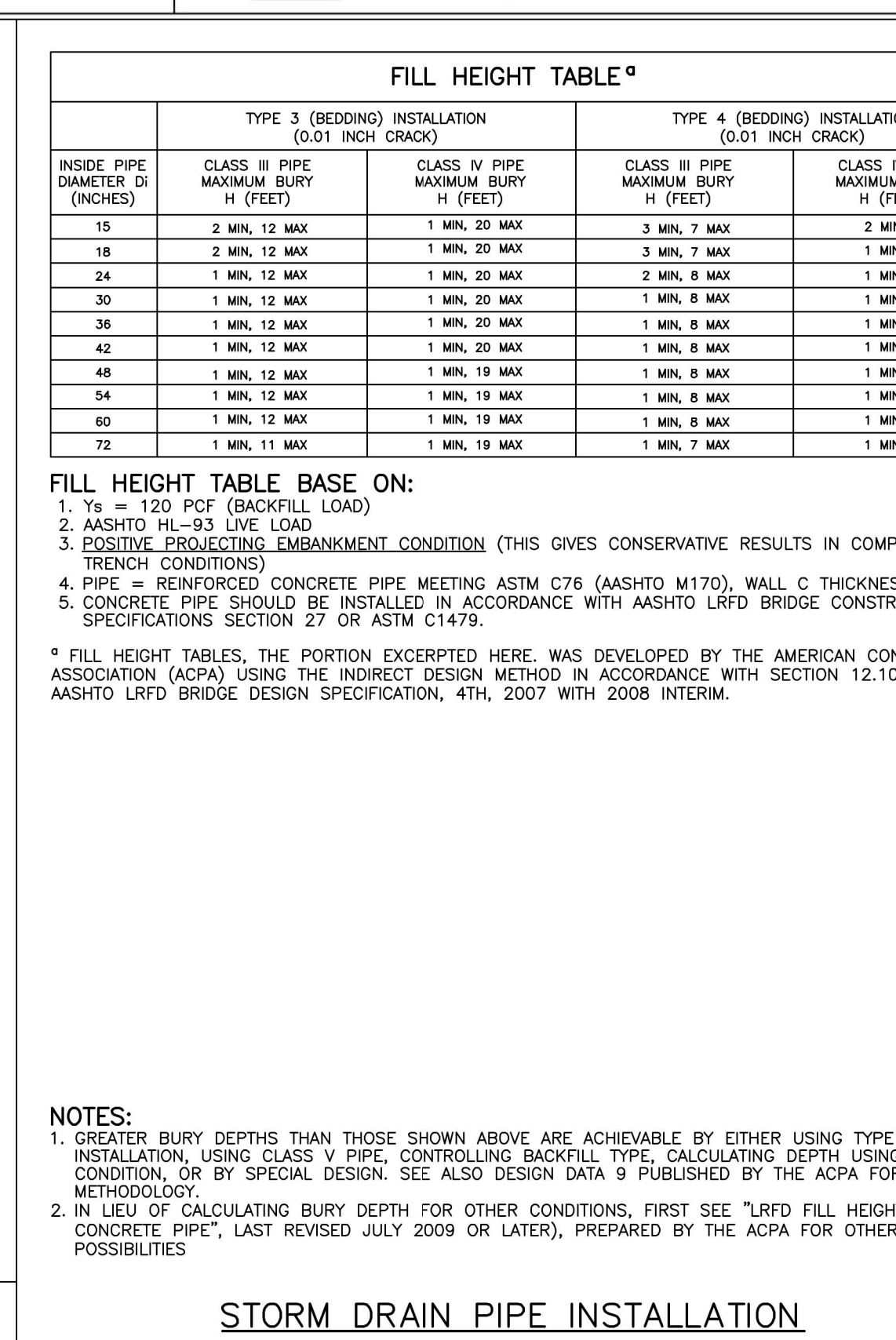
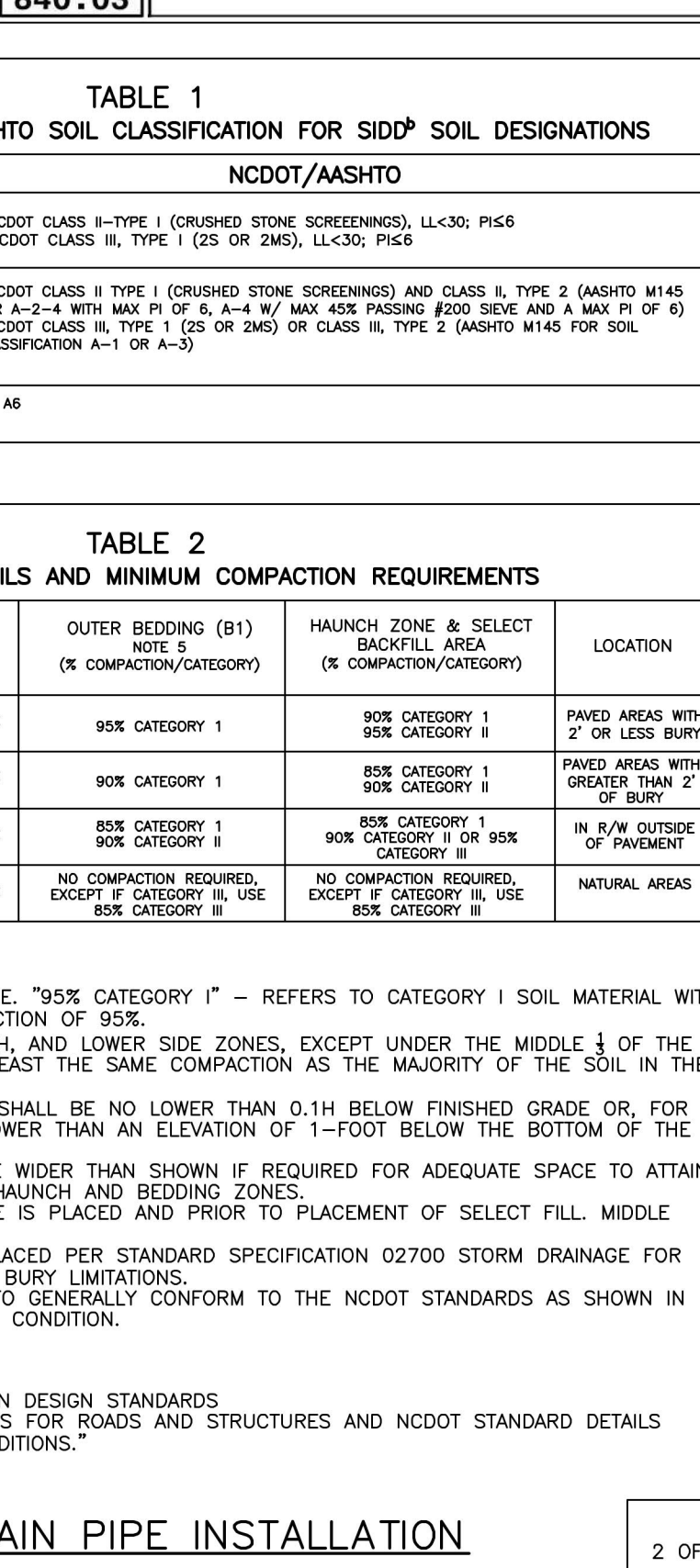
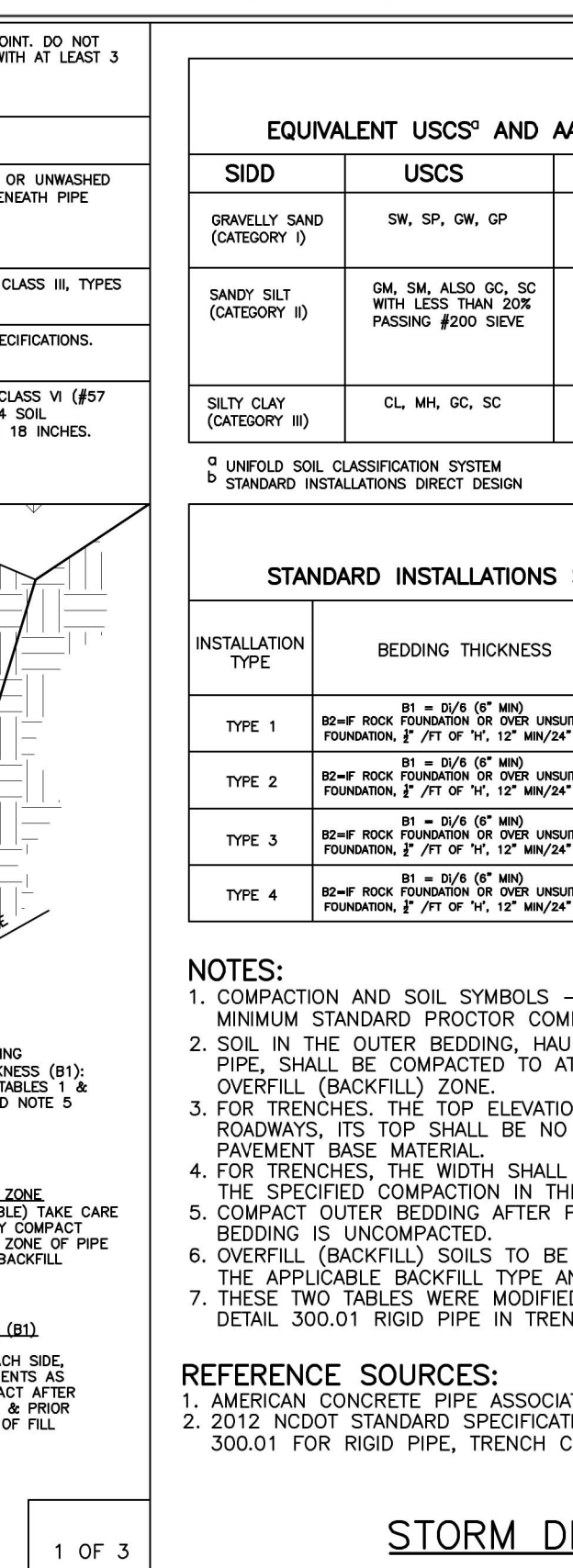
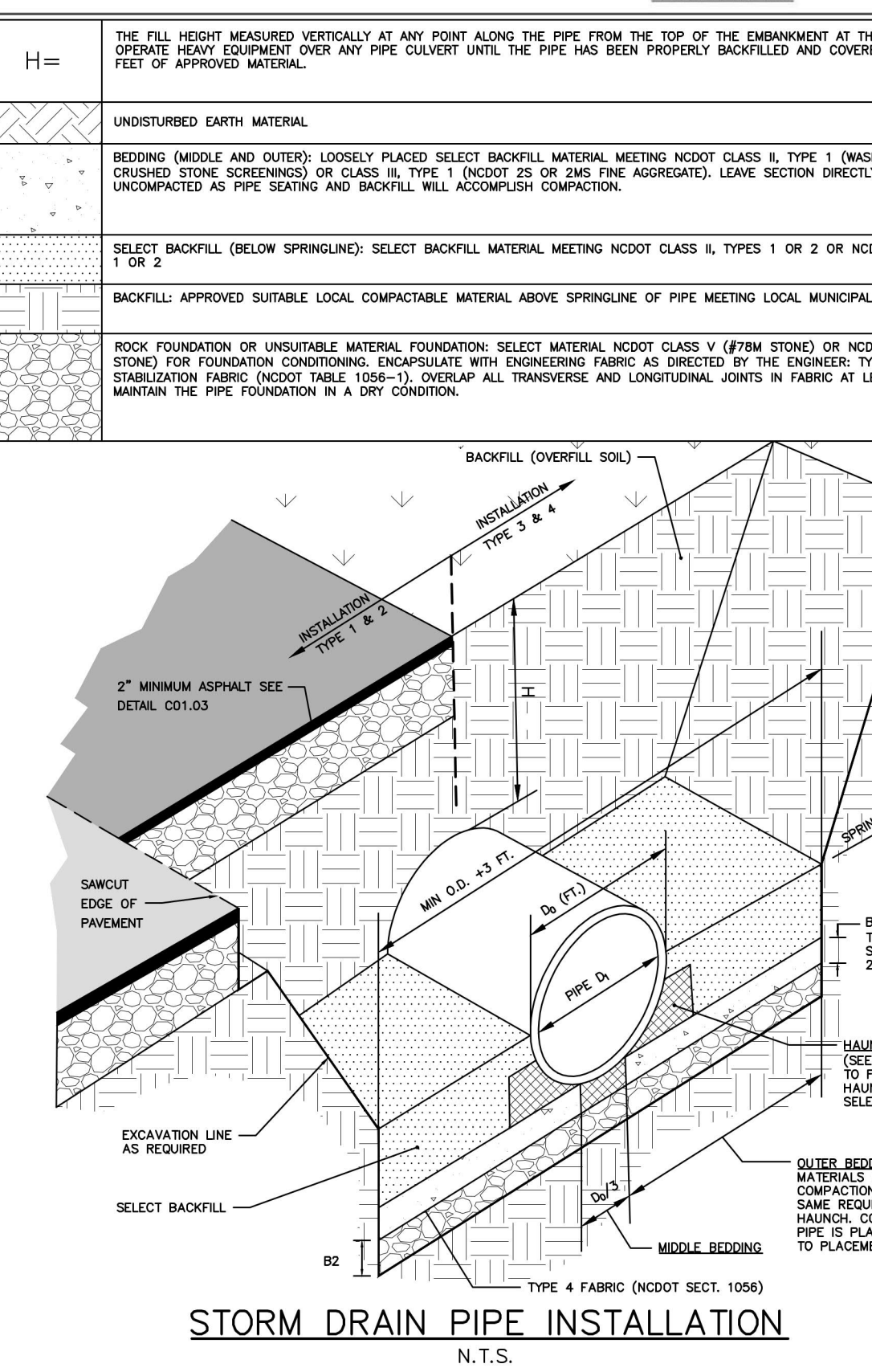
STATE OF NORTH CAROLINA DEPT. OF TRANSPORTATION DIVISION OF HIGHWAYS RALEIGH, N.C.



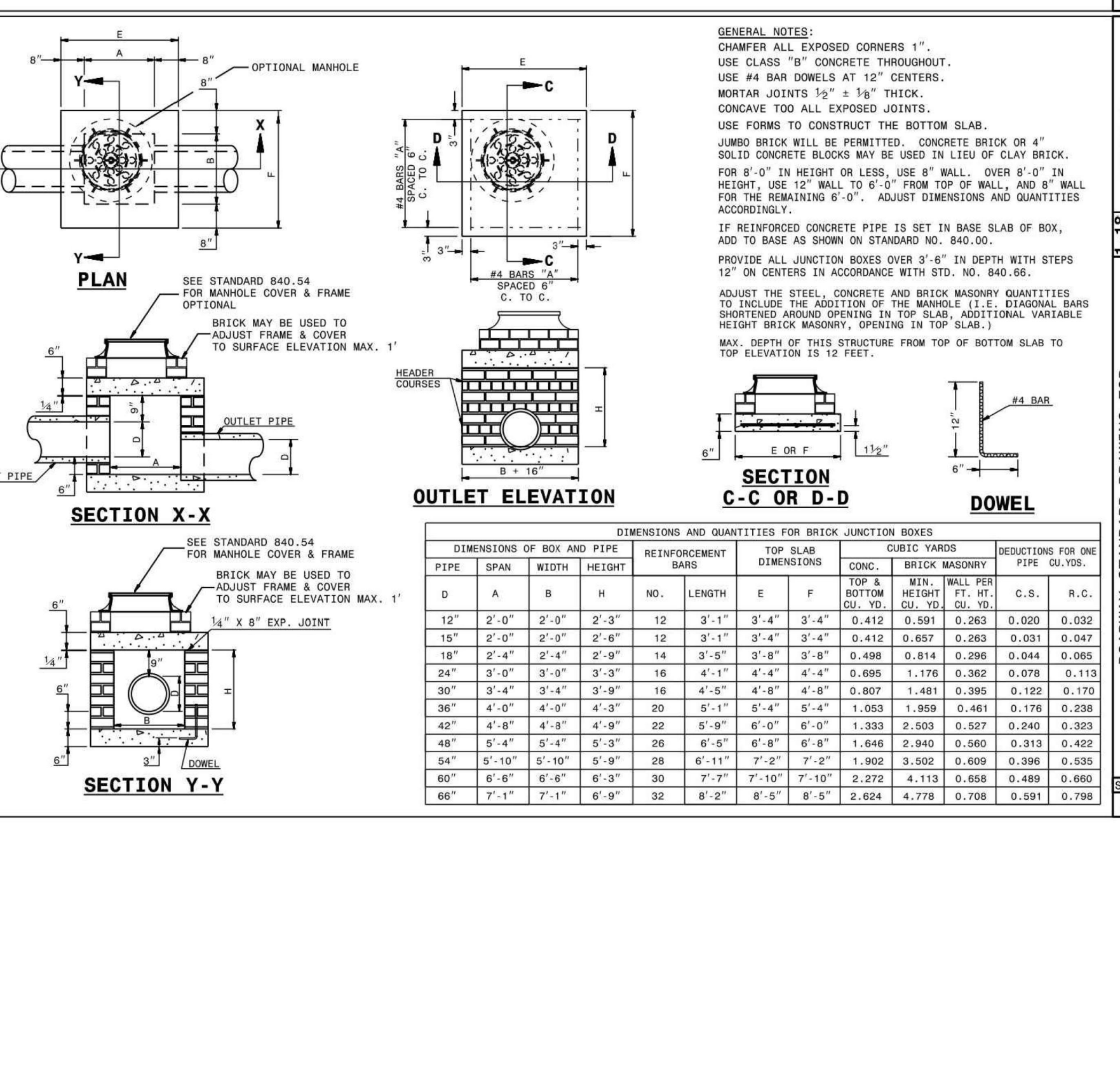
STATE OF NORTH CAROLINA DEPT. OF TRANSPORTATION DIVISION OF HIGHWAYS RALEIGH, N.C.



STATE OF NORTH CAROLINA DEPT. OF TRANSPORTATION DIVISION OF HIGHWAYS RALEIGH, N.C.



STATE OF NORTH CAROLINA DEPT. OF TRANSPORTATION DIVISION OF HIGHWAYS RALEIGH, N.C.



STATE OF NORTH CAROLINA DEPT. OF TRANSPORTATION DIVISION OF HIGHWAYS RALEIGH, N.C.

STORM DRAIN PIPE INSTALLATION
 N.T.S.
 1 OF 3

STORM DRAIN PIPE INSTALLATION
 N.T.S.
 2 OF 3

STORM DRAIN PIPE INSTALLATION
 N.T.S.
 3 OF 3

STORM DRAIN PIPE INSTALLATION
 N.T.S.
 4 OF 3

The John R. McAdams Company, Inc.
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CLIENT
 ASHTON RALEIGH RESIDENTIAL, LLC.
 900 RIDGEBFIELD DRIVE, SUITE 335
 RALEIGH, NORTH CAROLINA 27609
 PHONE: 919.422.7663
 CONTACT: BOB MISHLER

ASHTON WOODS

THE POINT
 PHASES 3, 4, 5, 7 AND 10
 CONSTRUCTION DRAWINGS - PACKAGE 2
 EAST YOUNG STREET
 TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
 WAKE COUNTY, NORTH CAROLINA

CD 20-08

William T. O'Daniel
 c/o-William T. O'Daniel, c/o-US, c/o-North Carolina
 email:williamodaniel@mcadamsco.com
 2023.05.05 15:26:40 -04'00'

REVISIONS
 NO. DATE

PLAN INFORMATION
 PROJECT NO. AWH-20000
 FILENAME AWH20000-CD-PKG-02-D1
 CHECKED BY
 DRAWN BY
 SCALE N.T.S.
 DATE 05.08.2023

SHEET
 STORM DRAINAGE
 DETAILS
 C8.02

FINAL DRAWING - RELEASED FOR CONSTRUCTION

GENERAL NOTES:

USE STANDARD OR JUMBO BRICK FOR WALL CONSTRUCTION. SOLID CONCRETE BRICK OR BLOCK ARE OPTIONAL WALL CONSTRUCTION MATERIAL IN THE UNIT PRICE BID FOR EACH UNIT.

INSTALL OPTIONAL MANHOLE IN POSITION AS DIRECTED BY THE ENGINEER. CUT OR BEND ALL REBAR CROSSING THIS OPENING TO ALLOW 2" MINIMUM CONCRETE COVERAGE. ENCLOSE THE OPENING WITH 8" A" BARS TIED TO THE REBAR MAT AND SET 50' A MINIMUM OF 3" CONCRETE COVER IS ATTAINED. REFERENCE STD. NO. 840.54 FOR MANHOLE INFORMATION.

PROVIDE JUNCTION BOXES WITH MANHOLES OVER 3'-6" IN DEPTH WITH STEPS PLACED ON 12" CENTERS. REFERENCE STD. NO. 840.66. SPACE DOWEL "C" BARS AT A MAXIMUM OF 12" CENTERS. MAXIMUM DEPTH OF THIS UNIT IS SHOWN IN 12".

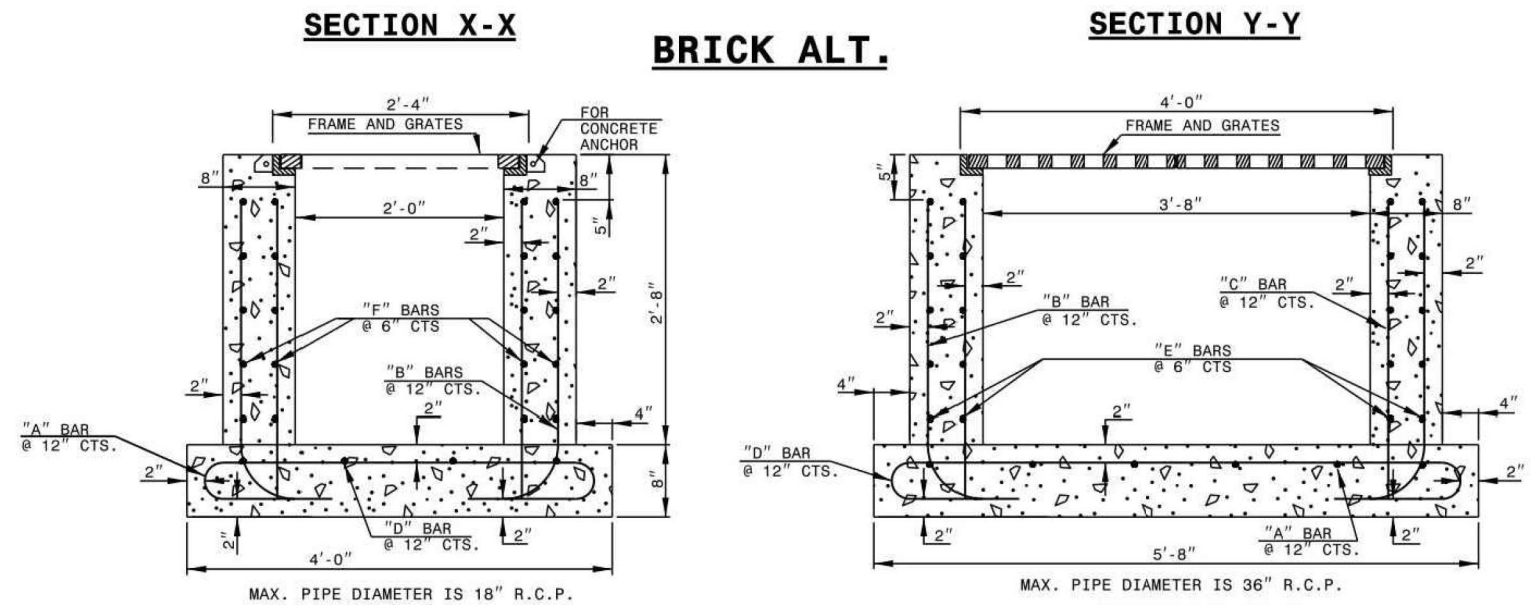
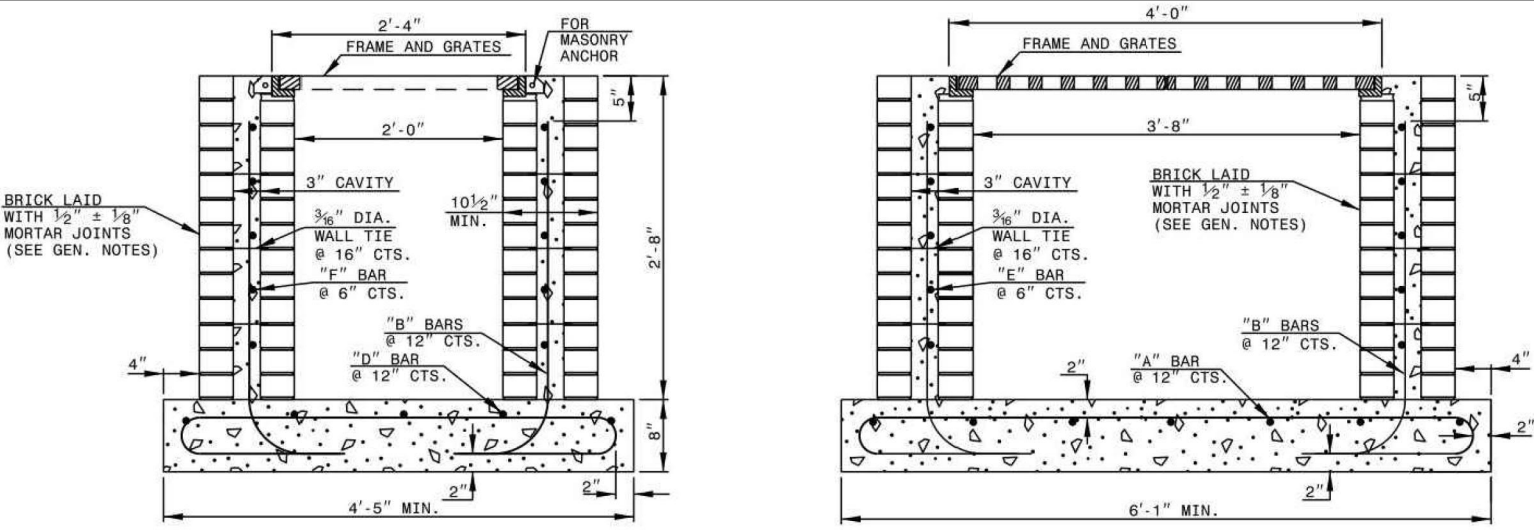
CONSTRUCT THE JUNCTION BOX IN ACCORDANCE WITH SECTIONS 830, 832, 834 AND 840 OF THE STANDARD SPECIFICATIONS.

BILL OF MATERIAL

Table with 5 columns: BAR, QTY, SIZE, LENGTH, WEIGHT. Lists materials for brick and concrete alternatives.

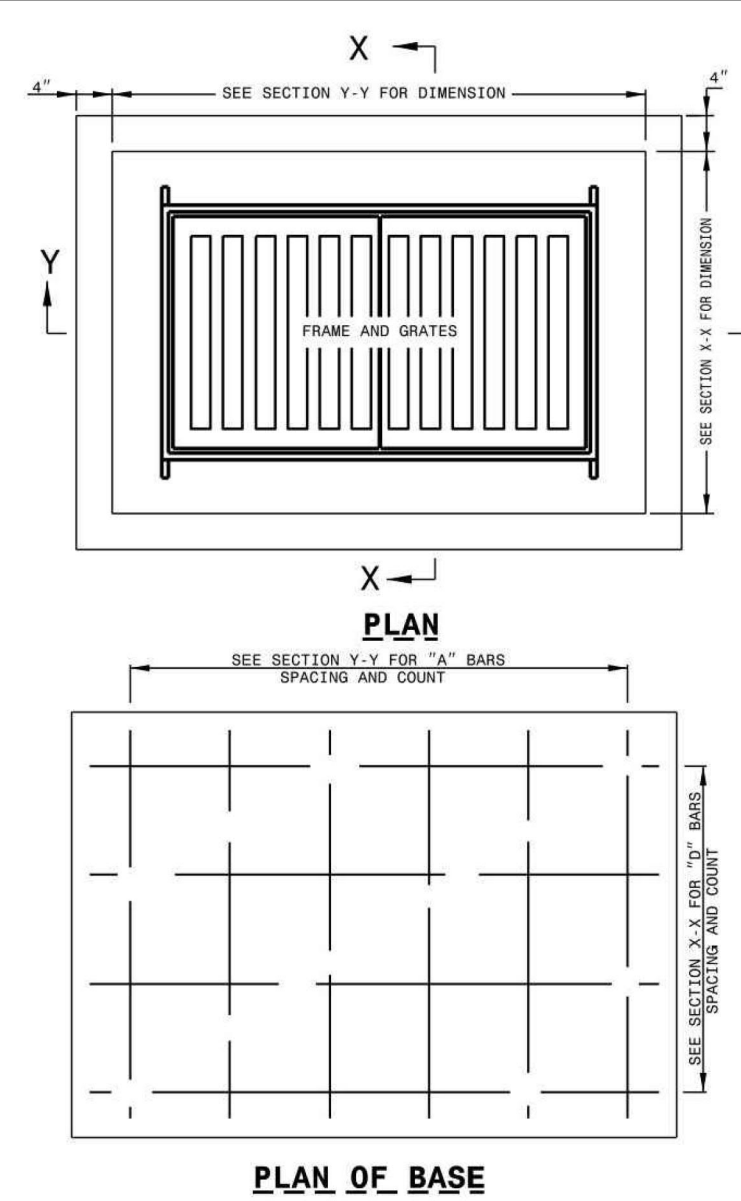
Table with 3 columns: QTY, YDS, CLASS "AA" CONC. Lists concrete quantities for brick and concrete alternatives.

STATE OF NORTH CAROLINA DEPT. DIVISION OF HIGHWAYS RALEIGH, N.C. ROADWAY STANDARD DRAWING FOR TRAFFIC BEARING JUNCTION BOX FOR USE WITH PIPES 42" AND UNDER

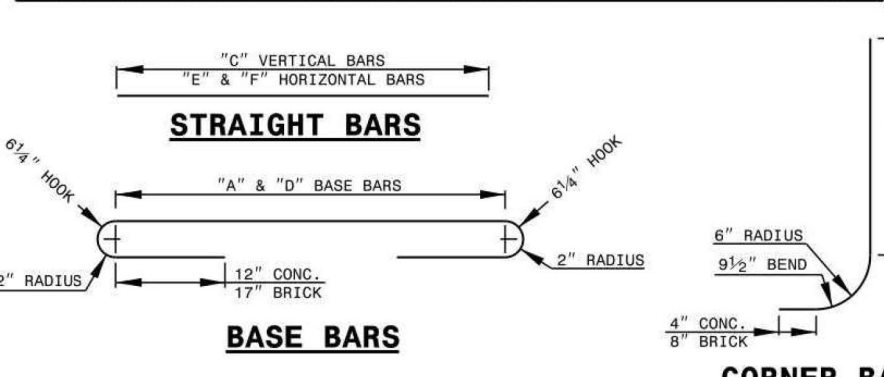


SHEET NO. OF 2 840.34

STATE OF NORTH CAROLINA DEPT. DIVISION OF HIGHWAYS RALEIGH, N.C. ROADWAY STANDARD DRAWING FOR TRAFFIC BEARING GRATED DROP INLET FOR CAST IRON DOUBLE FRAME AND GRATES



BILL OF MATERIALS table for a traffic bearing grated drop inlet, listing common and concrete materials.



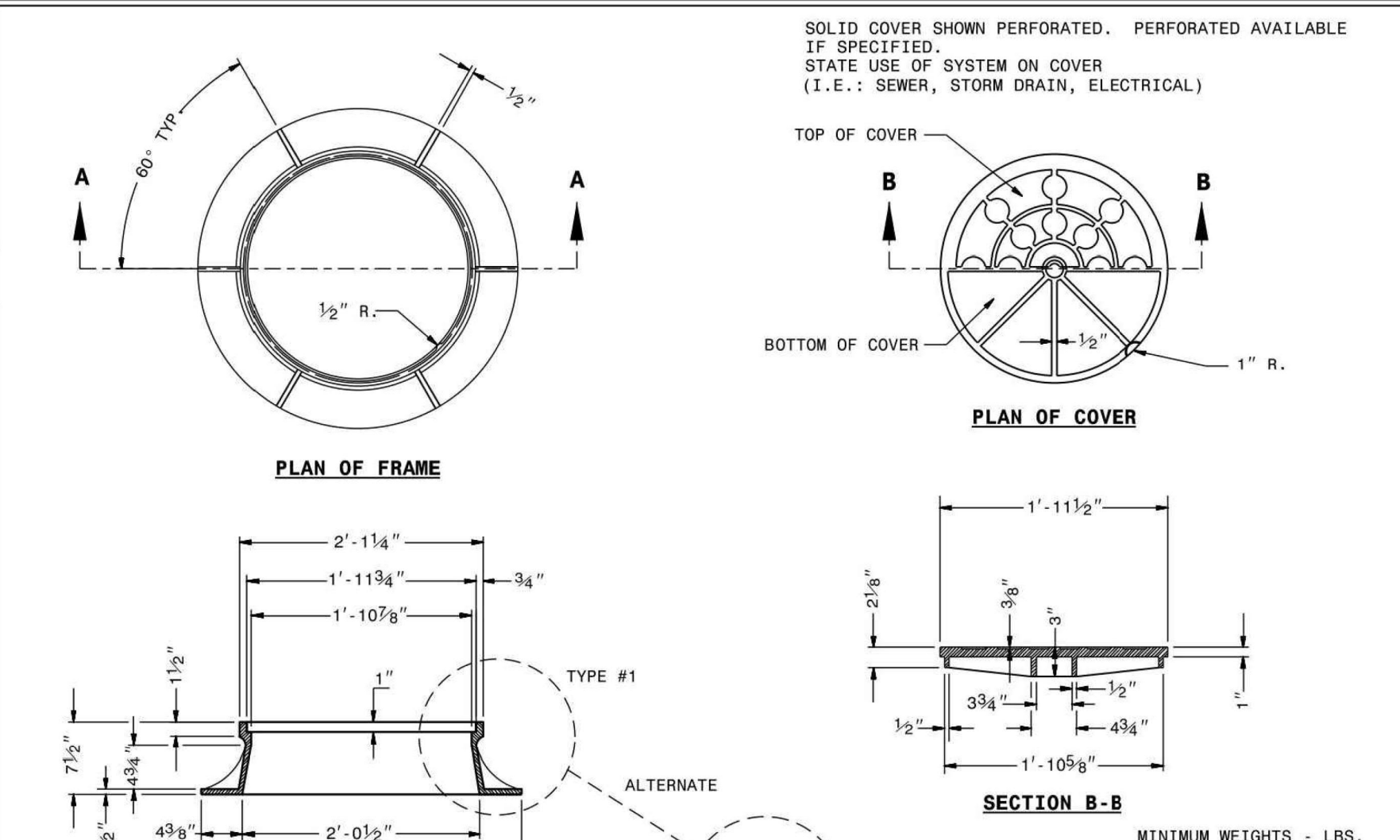
GENERAL NOTES: USE CLASS 'AA' CONCRETE FOR CAST IN PLACE CONCRETE BOX. USE CLASS 'B' CONCRETE IN THE WALL CAVITY FOR REINFORCED BRICK CONSTRUCTION AND CLASS 'AA' FOR THE FOOTING BASE.

SHEET NO. OF 2 840.35

STATE OF NORTH CAROLINA DEPT. DIVISION OF HIGHWAYS RALEIGH, N.C. ROADWAY STANDARD DRAWING FOR TRAFFIC BEARING GRATED DROP INLET FOR CAST IRON DOUBLE FRAME AND GRATES

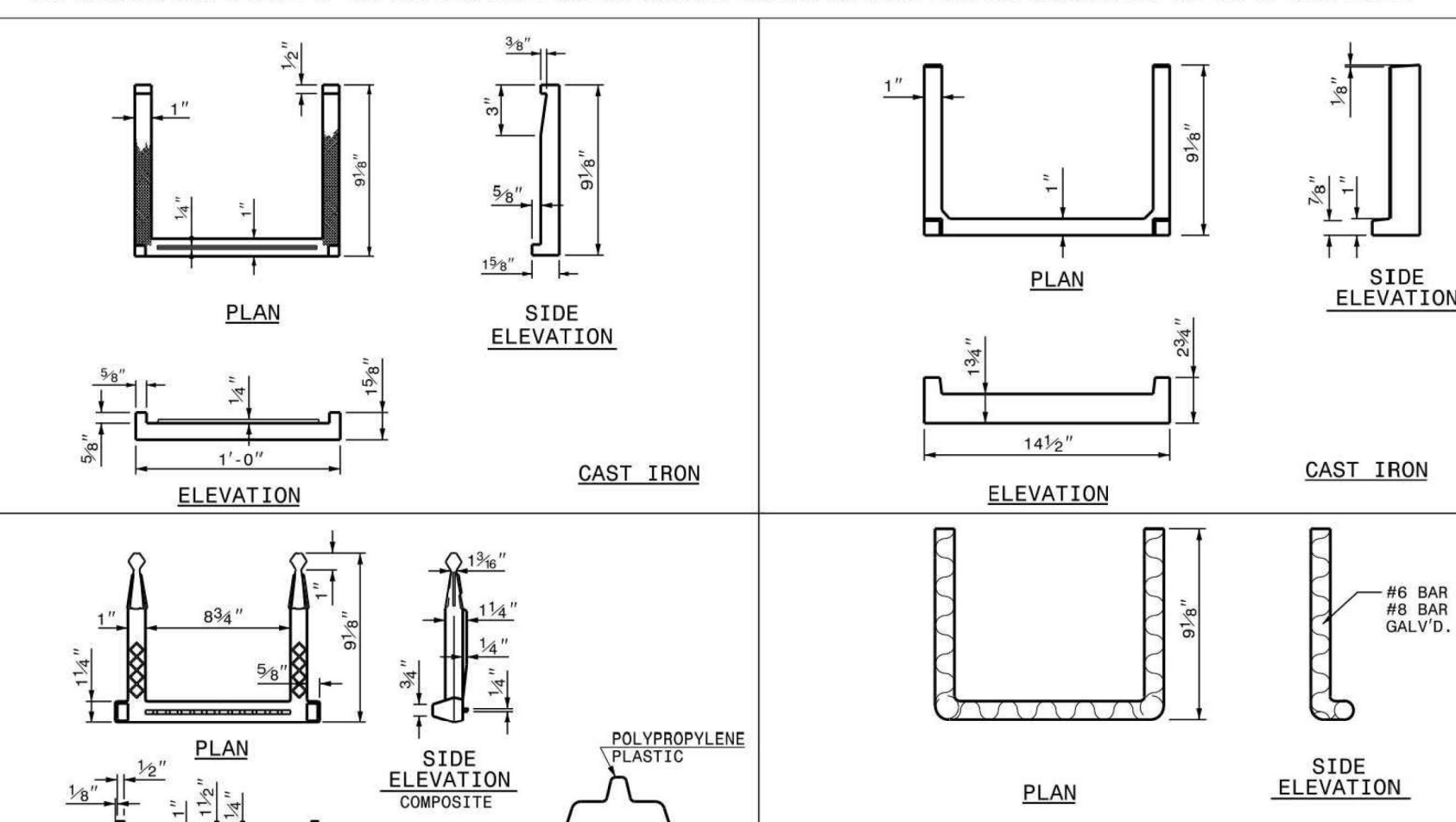
SHEET NO. OF 2 840.35

STATE OF NORTH CAROLINA DEPT. DIVISION OF HIGHWAYS RALEIGH, N.C. ROADWAY STANDARD DRAWING FOR MANHOLE FRAME AND COVER



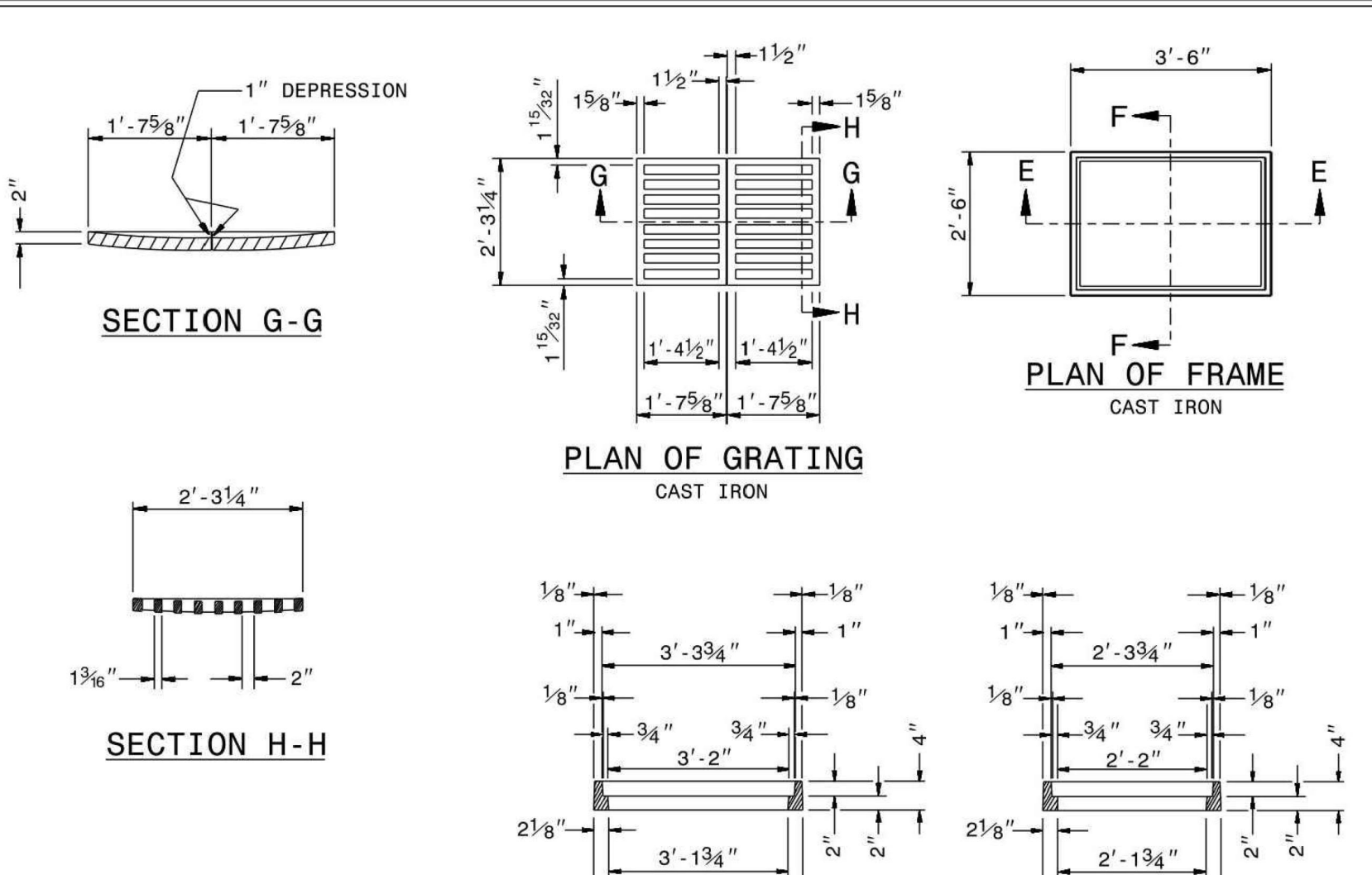
SHEET 1 OF 1 840.54

NOTES: INSTALL ALL STEPS PROTRUDING 4" FROM INSIDE FACE OF STRUCTURE WALL. STEPS DIFFERING IN DIMENSIONS, CONFIGURATION, OR MATERIALS THOSE SHOWN MAY ALSO BE USED PROVIDED THE CONTRACTOR HAS FURNISHED THE ENGINEER WITH DETAILS OF THE PROPOSED STEPS AND HAS RECEIVED WRITTEN APPROVAL FROM THE ENGINEER FOR THE USE OF SUCH STEPS.



STATE OF NORTH CAROLINA DEPT. DIVISION OF HIGHWAYS RALEIGH, N.C. ROADWAY STANDARD DRAWING FOR DRAINAGE STRUCTURE STEPS

SHEET 1 OF 1 840.66



STATE OF NORTH CAROLINA DEPT. DIVISION OF HIGHWAYS RALEIGH, N.C. ROADWAY STANDARD DRAWING FOR DROP INLET FRAME AND GRATES FOR USE WITH STD. DWG. S 840.14 AND 840.15

SHEET 1 OF 1 840.16

Section 2721 Engineered Surface Drainage Products. Includes general notes, materials, and installation instructions for drainage products.

NON TRAFFIC INSTALLATION. Includes diagrams for drain basin and inline drain with detailed notes and a table of grate options.

NYLOPLAST 12" DRAIN BASIN: 2812AG __ X. Includes detailed technical drawings and a table of grate options.

NYLOPLAST 15" DRAIN BASIN: 2815AG __ X. Includes detailed technical drawings and a table of grate options.

NYLOPLAST 18" DRAIN BASIN: 2818AG __ X. Includes detailed technical drawings and a table of grate options.

NYLOPLAST 15" DRAIN BASIN: 2815AG __ X. Includes detailed technical drawings and a table of grate options.

NYLOPLAST 18" DRAIN BASIN: 2818AG __ X. Includes detailed technical drawings and a table of grate options.

Reinforced concrete lintel. Includes cross-section diagram and technical notes for storm pipe through wall detail.

WALL DRAIN INLET DETAIL. Includes cross-section diagram showing the integration of the drain inlet with a retaining wall.

US FOUNDRY Heavy Duty Grey Cast Iron ASTM-A18 Class 35B. Includes technical drawings and product specifications.

McADAMS logo and contact information: The John R. McAdams Company, Inc., 2905 Meridian Parkway, Durham, NC 27713.

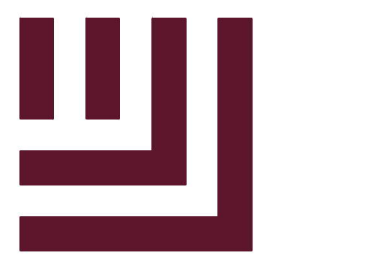
ASHTON WOODS logo and contact information: ASHTON RALEIGH RESIDENTIAL, LLC, 900 RIDGECREST DRIVE, SUITE 335, RALEIGH, NORTH CAROLINA 27609.

THE POINT PHASES 3, 4, 5, 7 AND 10 CONSTRUCTION DRAWINGS - PACKAGE 2 EAST YOUNG STREET TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP, WAKE COUNTY, NORTH CAROLINA.

CD 20-08 seal and signature block with professional engineer details.

REVISIONS table and PLAN INFORMATION section including project number, filename, and date.

STORM DRAINAGE DETAILS C8.03 FINAL DRAWING - RELEASED FOR CONSTRUCTION



McADAMS

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PHONE: 919. 422. 7663
CONTACT: BOB MISHLER



THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08

CD 20-08



William T. O'Daniel
Professional Engineer
Seal 22630
W.T. O'Daniel

REVISIONS

NO. DATE

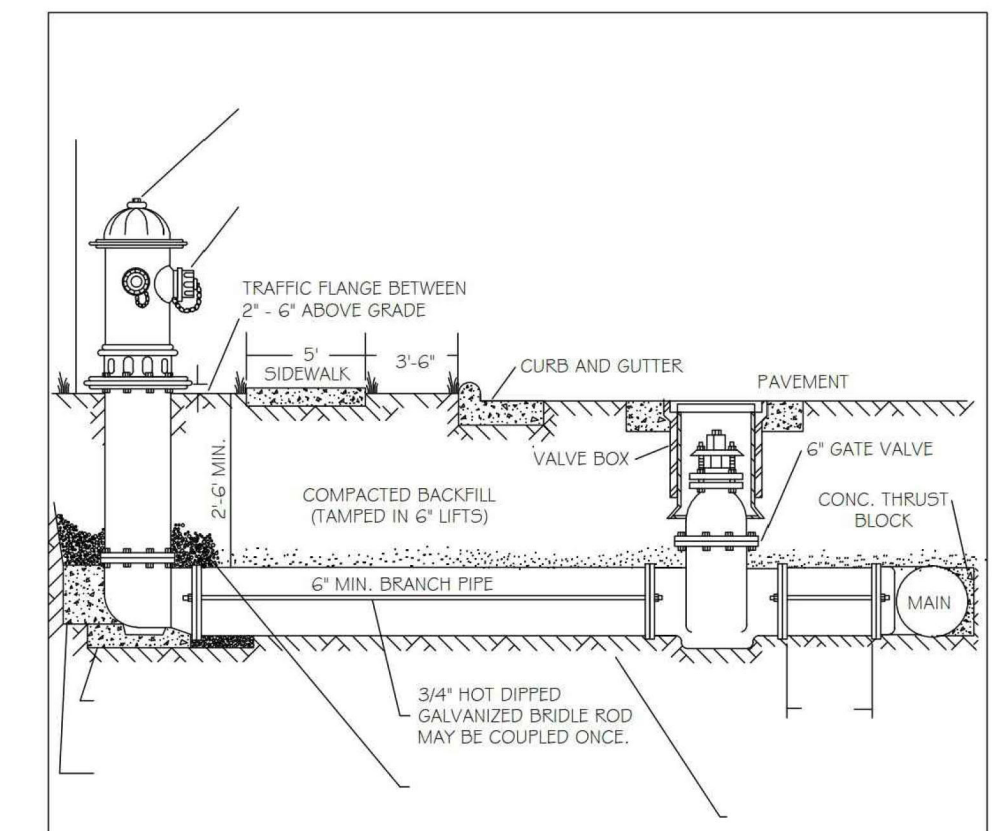
PLAN INFORMATION

PROJECT NO. AWH-20000
FILENAME AWH20000-CD-PKG-02-D1
CHECKED BY
DRAWN BY
SCALE N.T.S.
DATE 05.08.2023
SHEET

WATER DETAILS

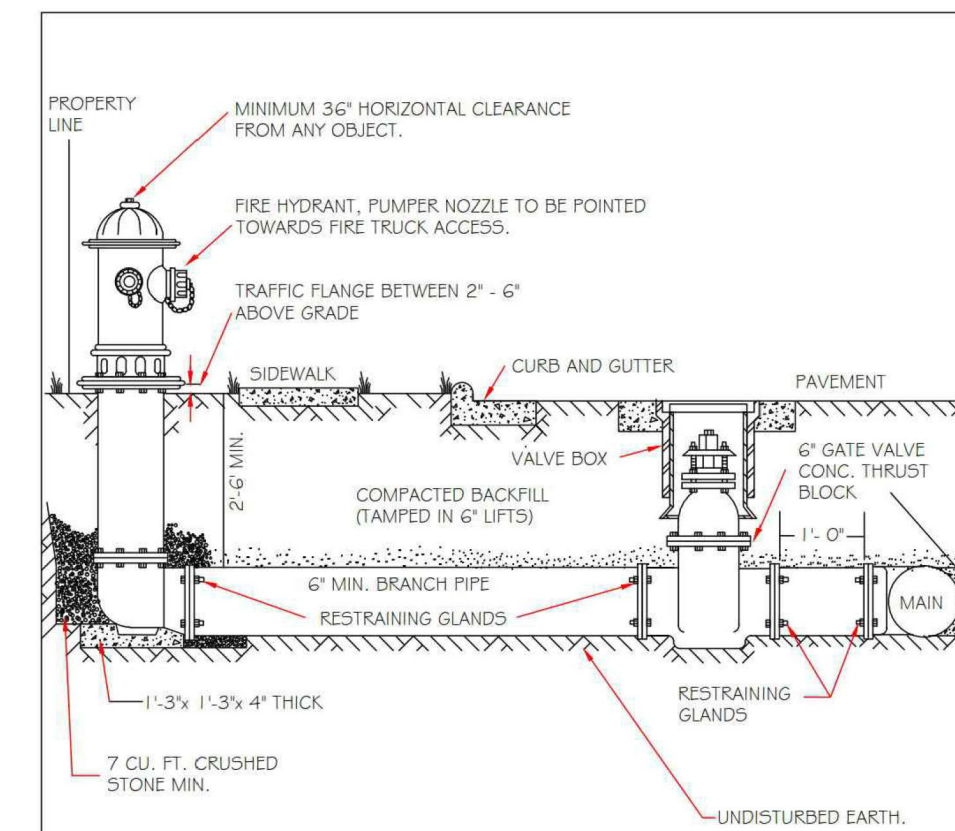
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FINAL DRAWING - RELEASED FOR CONSTRUCTION



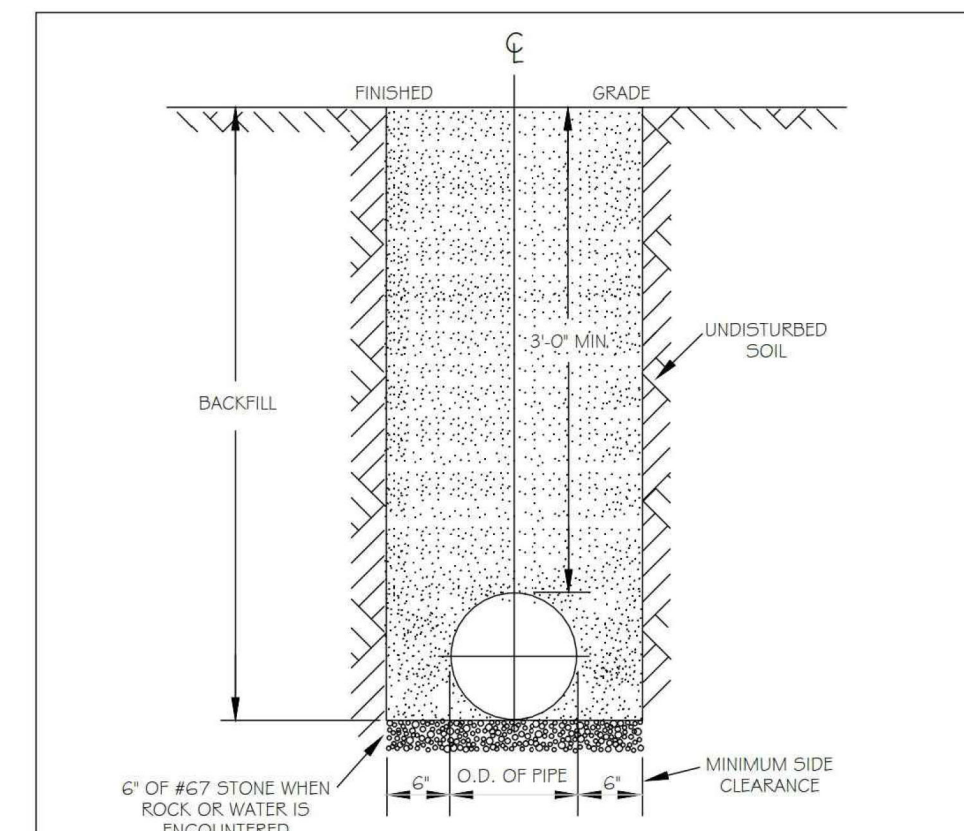
NOTES:
1. FIRE HYDRANT SHALL BE AS MANUFACTURED: MUELLER, AMERICAN DARLING, KENNEDY, MPH, WATERLOO, GUNN, EAST JORDAN IRON WORKS, OR ICP PIPE.
2. BRANCH PIPE SHALL BE DUCTILE IRON AWWA C150-96.
3. 6" GATE VALVE SHALL BE AWWA C300-96 OPEN LEFT.
4. STEEL RODS AND BOLTS SHALL BE 3/4" HOT DIPPED GALVANIZED.
5. FIRE HYDRANTS WILL BE INSTALLED IN TRUE VERTICAL POSITION.
6. FIRE HYDRANTS SHALL BE INSTALLED WITH A MECHANICAL RESTRAINING GLAND WITH A REBAR CAGE SHALL BE INSTALLED NO MORE THAN 10" FROM HYDRANT AND POURED IN CONCRETE.
7. FIRE HYDRANTS TO BE LOCATED IN ROW OR 2 FOOT EASEMENT ADJACENT TO ROW.

Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-4.



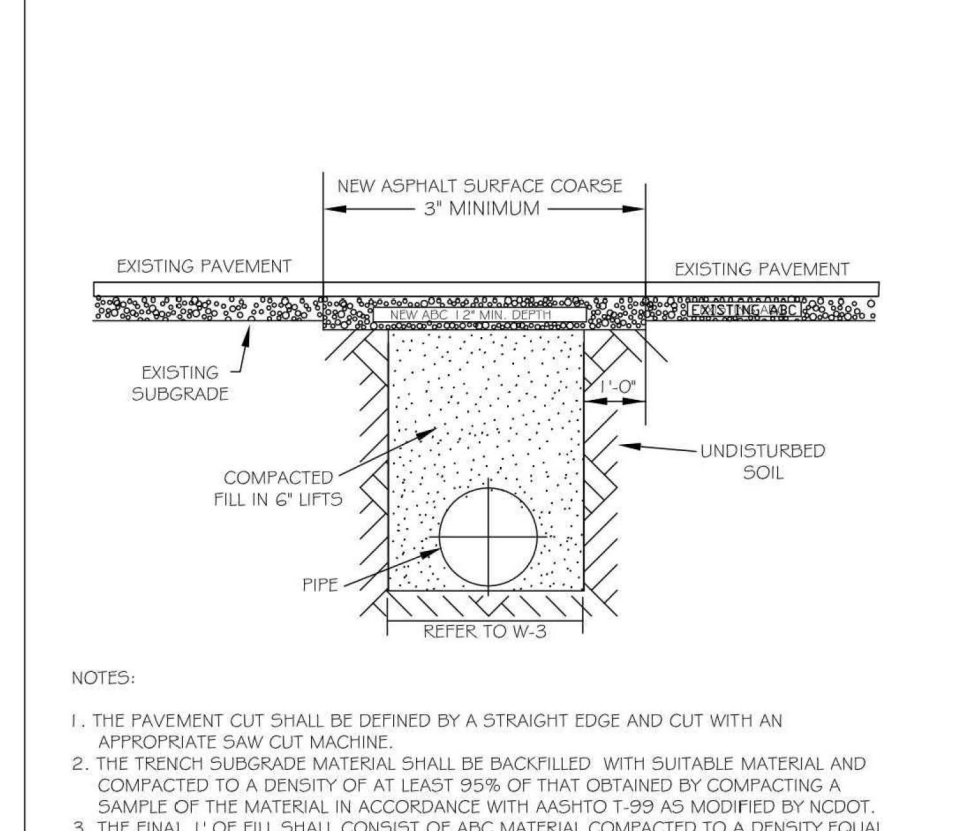
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Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-4.



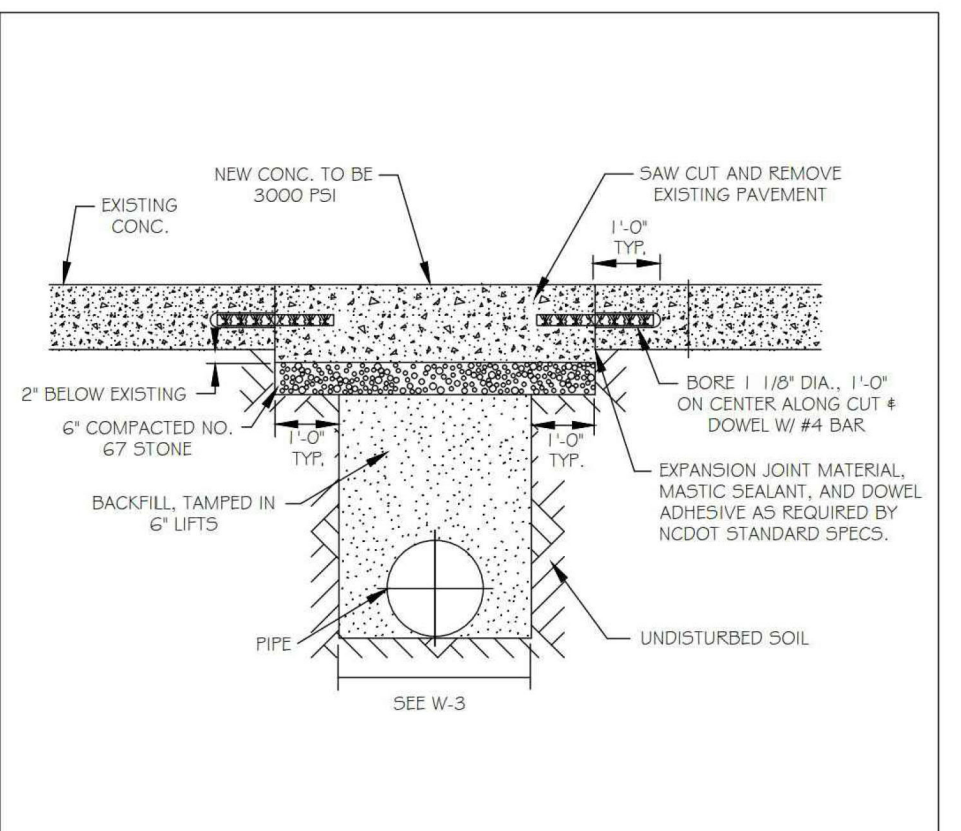
NOTES:
1. TRENCHES REQUIRING SHORING AND BRACING, DIMENSIONS SHALL BE TAKEN FROM THE INSIDE FACE OF THE SHORING AND BRACING.
2. NO RODS OR BOLLERS 4" OR LARGER TO BE USED IN BACKFILL.
3. ALL BACKFILL MATERIAL SHALL BE SUITABLE NATIVE MATERIAL.
4. BACKFILL SHALL BE TAMPED IN 6" LIFTS.
5. ACHIEVE 95% COMPACTION IN BACKFILL.

Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-3.



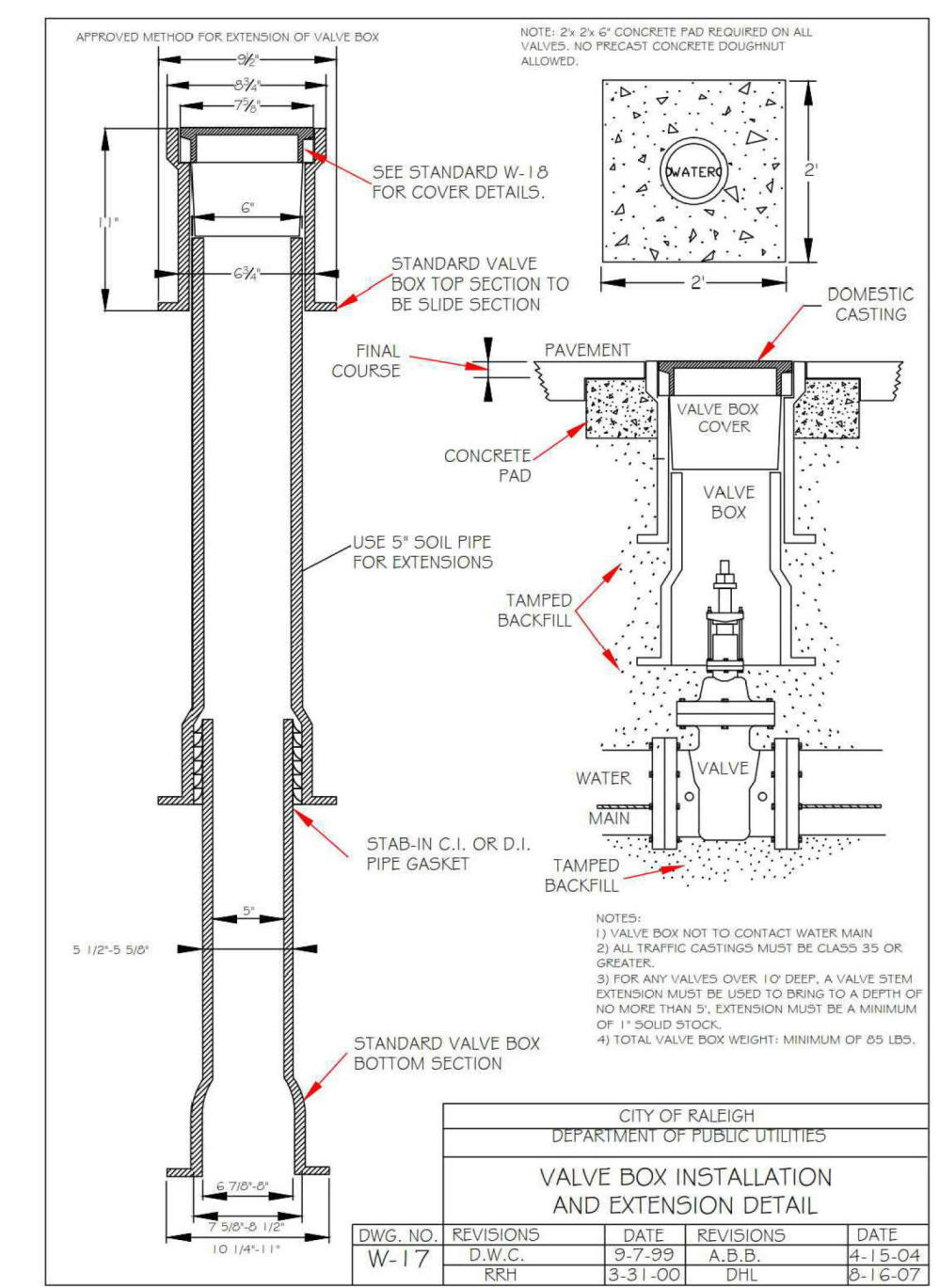
NOTES:
1. THE PAVEMENT CUT SHALL BE DEFINED BY A STRAIGHT EDGE AND CUT WITH AN APPROPRIATE SAW CUT MACHINE.
2. THE TRENCH SUBGRADE MATERIAL SHALL BE BACKFILLED WITH SUITABLE MATERIAL AND COMPACTED TO A DENSITY OF AT LEAST 95% OF THAT OBTAINED BY COMPACTING A SAMPLE OF THE MATERIAL IN ACCORDANCE WITH AASHTO T-99 AS MODIFIED BY NC DOT.
3. THE FINAL 1" OF FILL SHALL CONSIST OF ABC MATERIAL COMPACTED TO A DENSITY EQUAL TO 100% OF THAT OBTAINED BY COMPACTING A SAMPLE OF THE MATERIAL IN ACCORDANCE WITH AASHTO T-99 AS MODIFIED BY NC DOT.

Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-2.



NOTES:
1. SEE CITY OF RALEIGH STANDARDS FOR TRENCHES AND PIPE BEDDING W-3 FOR ADDITIONAL DETAILS.
2. PAVEMENT CUTS WITHIN NC DOT ROW SHALL CONFORM TO THE APPROVED ON SITE ENCROACHMENT PERMIT.
3. THE PAVEMENT CUT SHALL BE DEFINED BY A STRAIGHT EDGE AND CUT WITH AN APPROVED SAW CUT MACHINE.
4. THE TRENCH SUBGRADE MATERIAL SHALL BE BACKFILLED WITH SUITABLE MATERIAL AND COMPACTED TO A DENSITY OF AT LEAST 95% OF THAT OBTAINED BY COMPACTING A SAMPLE OF THE MATERIAL IN ACCORDANCE WITH AASHTO T-99 AS MODIFIED BY NC DOT.
5. THE FINAL 1" OF FILL SHALL CONSIST OF ABC MATERIAL COMPACTED TO A DENSITY EQUAL TO 100% OF THAT OBTAINED BY COMPACTING A SAMPLE OF THE MATERIAL IN ACCORDANCE WITH AASHTO T-99 AS MODIFIED BY NC DOT.

Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-1.



NOTES:
1. VALVE BOX NOT TO CONTACT WATER MAIN.
2. EQUAL TRAFFIC COUNTERS MUST BE 6" MIN. OR DEEPER.
3. FOR ANY VALVES OVER 10" DEEP, A VALVE SETTING EXTENSION MUST BE USED TO BRING TO A DEPTH OF NO MORE THAN 5" EXTENSION MUST BE A MINIMUM OF 1" OVER ROD.
4. TOTAL VALVE BOX HEIGHT: MINIMUM OF 60" (60" TOTAL).

Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-17.

REACTION BEARING AREAS FOR HORIZONTAL WATER PIPE BENDS. Table with columns: Bend Angle, Pipe Size, and Reaction Area.

THURST BLOCKING DESIGN QUANTITY TABLE. Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-10.

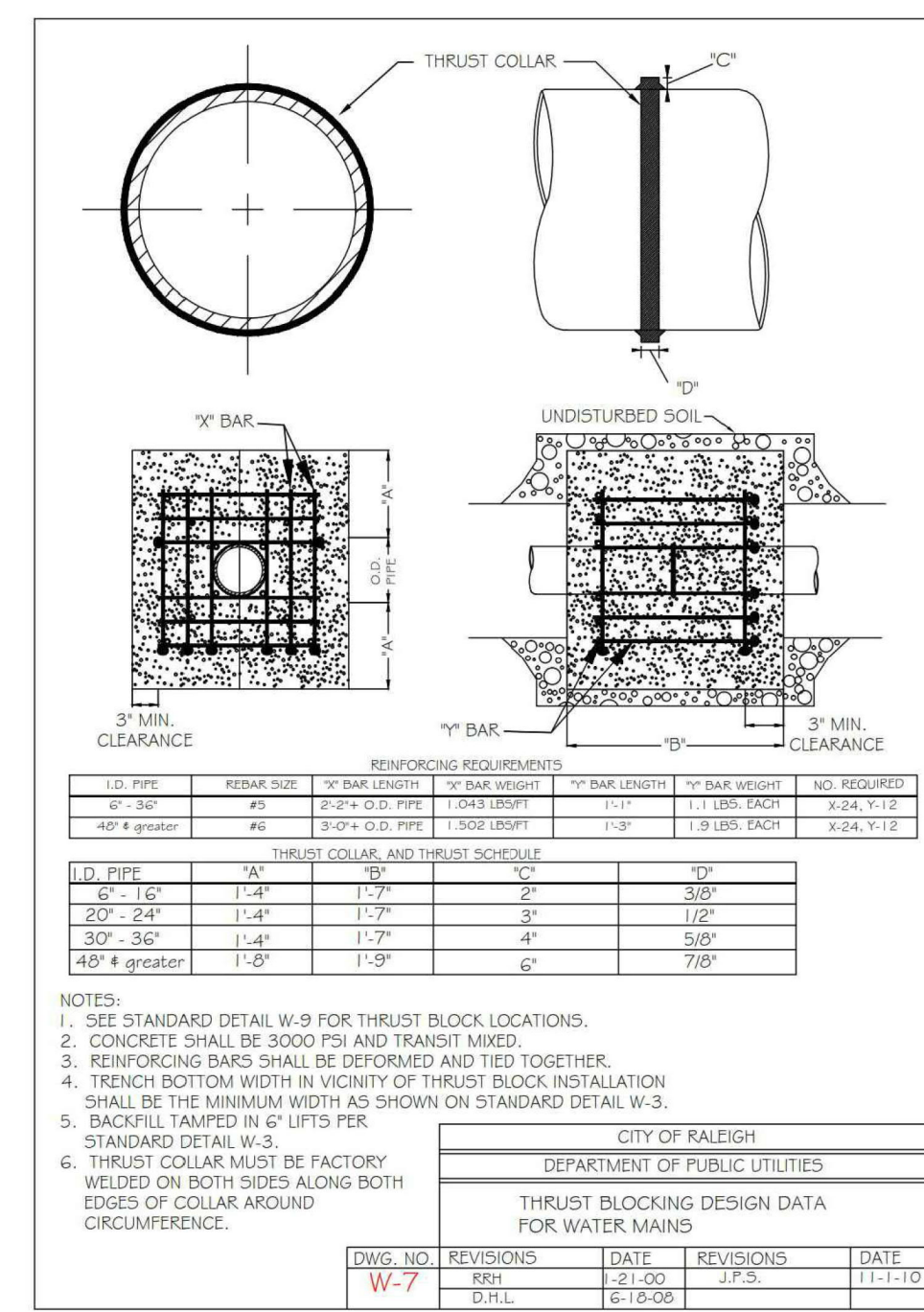


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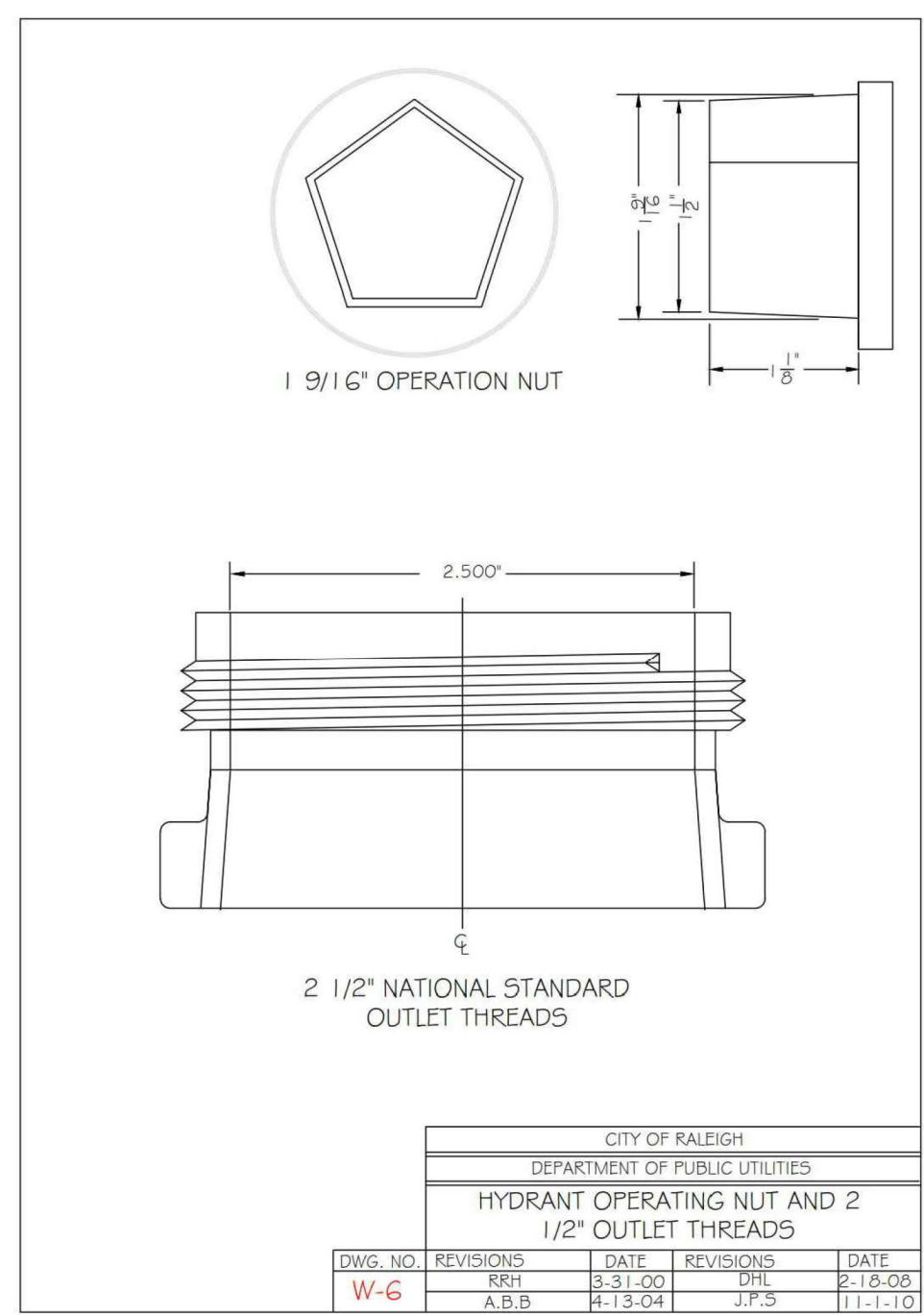


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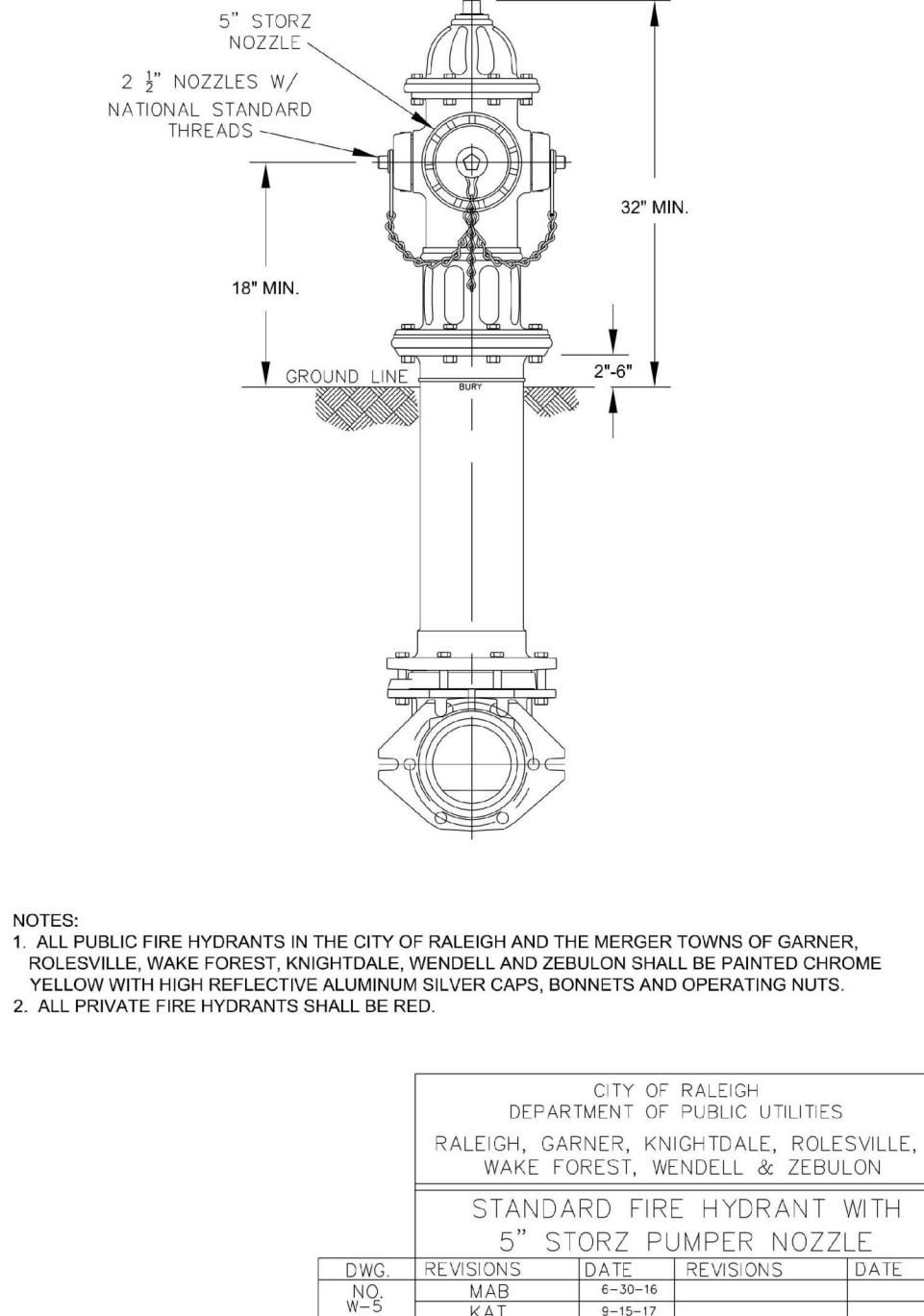


Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-5.

CONCURRENT REVIEW APPROVAL
CITY OF RALEIGH - PLANS AUTHORIZED FOR CONSTRUCTION
Plans for the proposed use have been reviewed for general compliance with applicable codes. This limited review, and authorization for construction is not to be considered to represent total compliance with all legal requirements for development and construction. The property owner, design consultants, and contractors are each responsible for compliance with all applicable City, State and Federal laws. This specific authorization below is not a permit, nor shall it be construed to permit any violation of City, State or Federal Law. All construction must be in accordance with all Local, State, and Federal Rules and Regulations.
Public Utilities Timothy Beasley

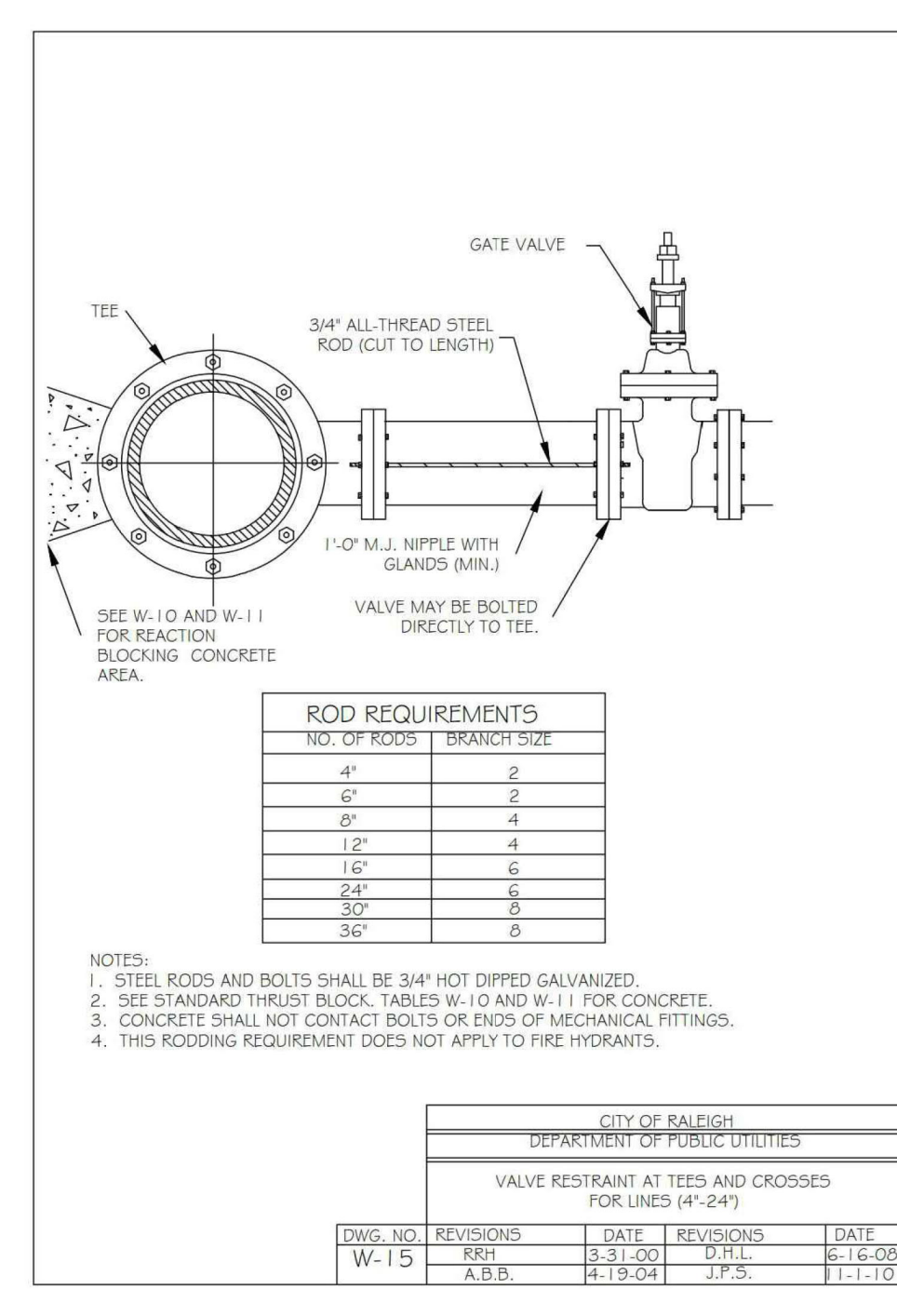


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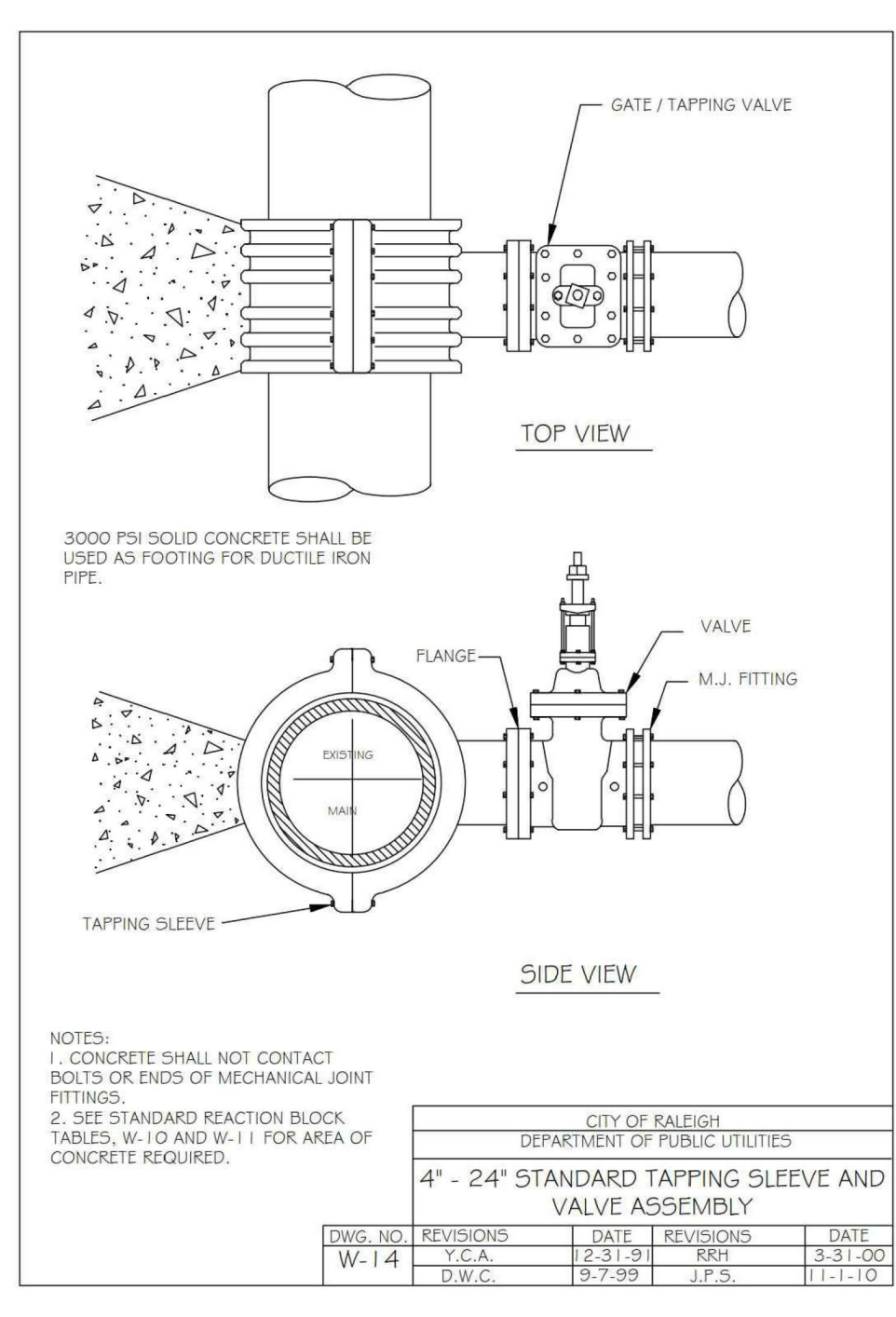
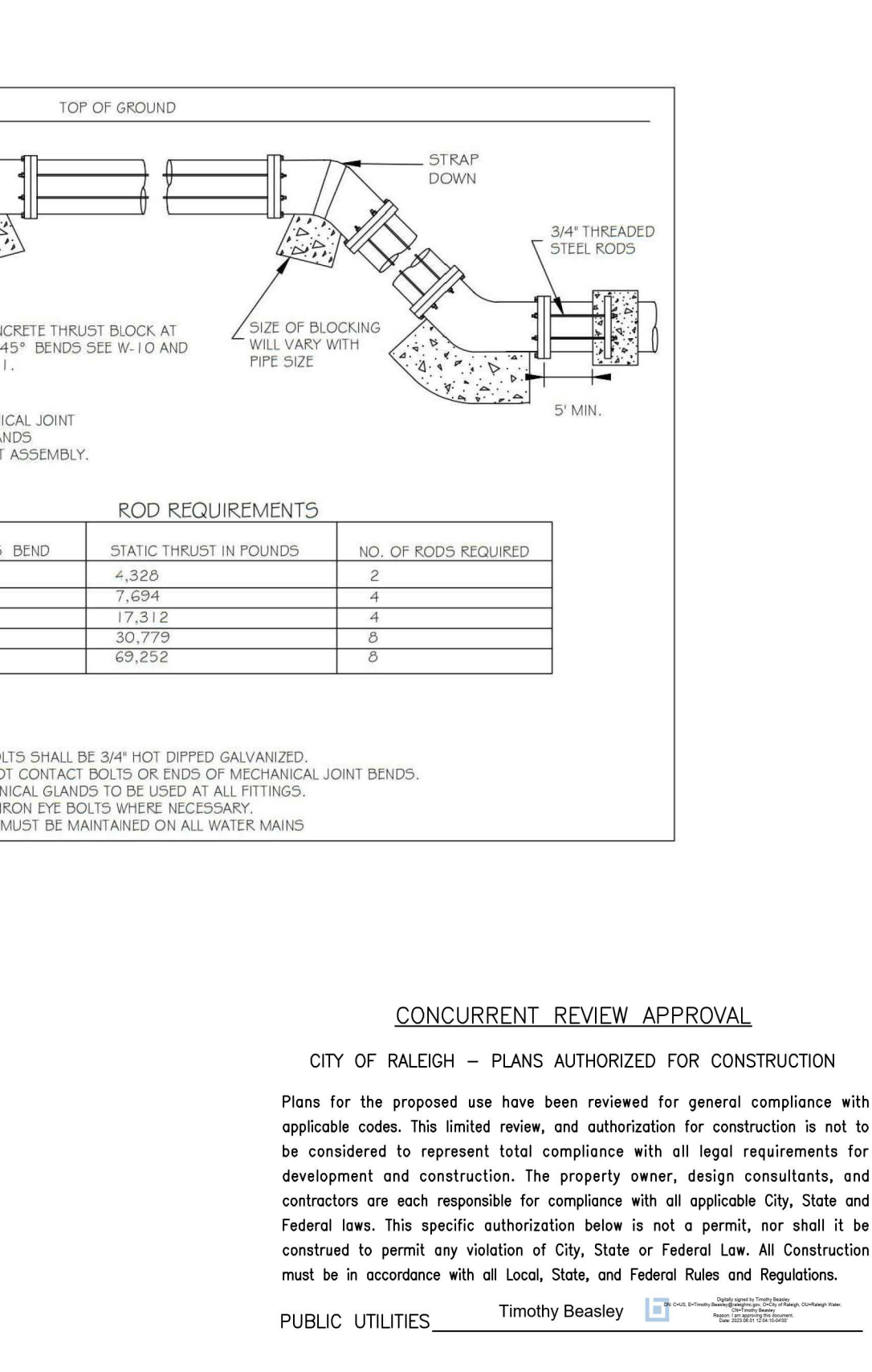
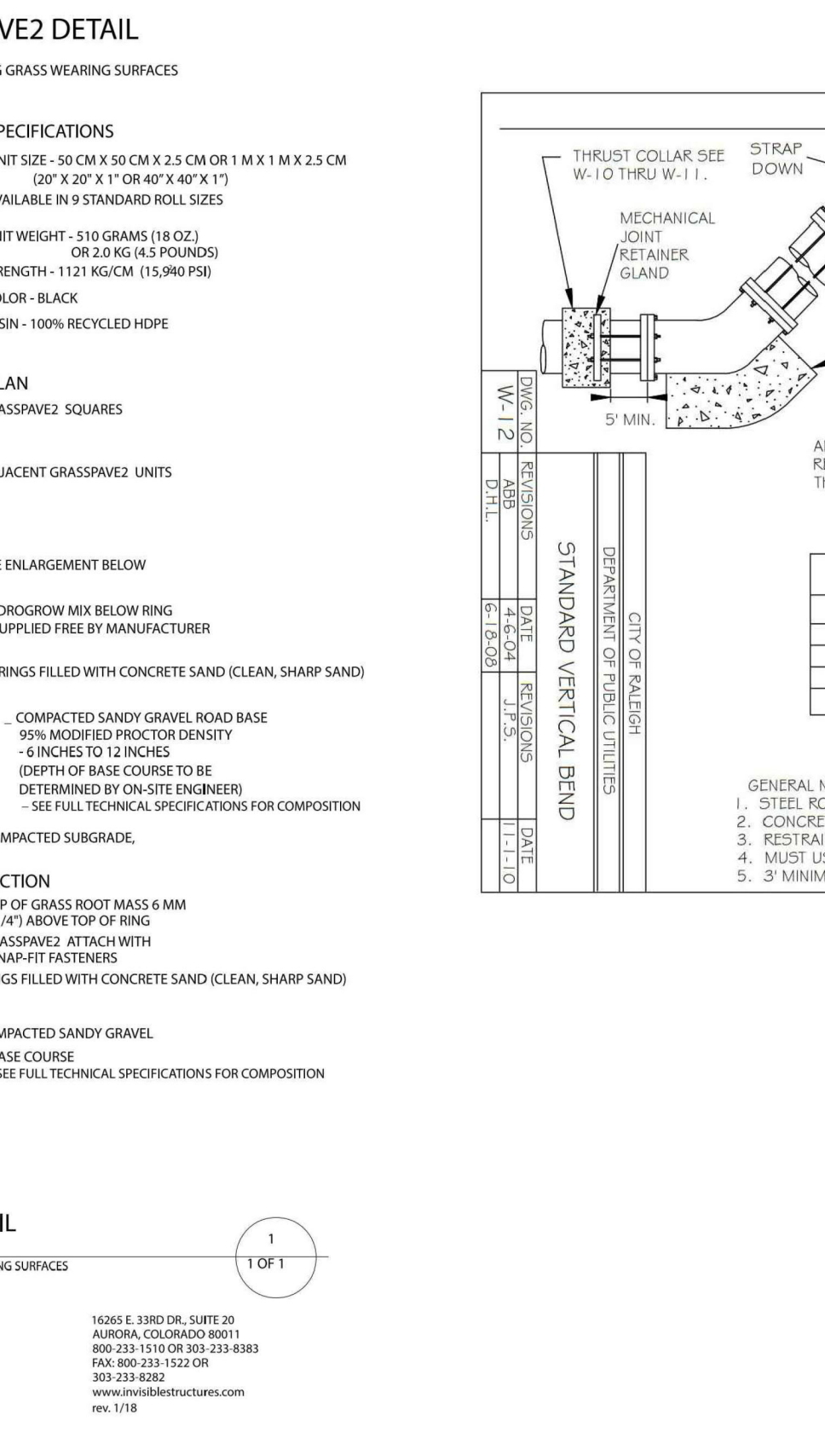
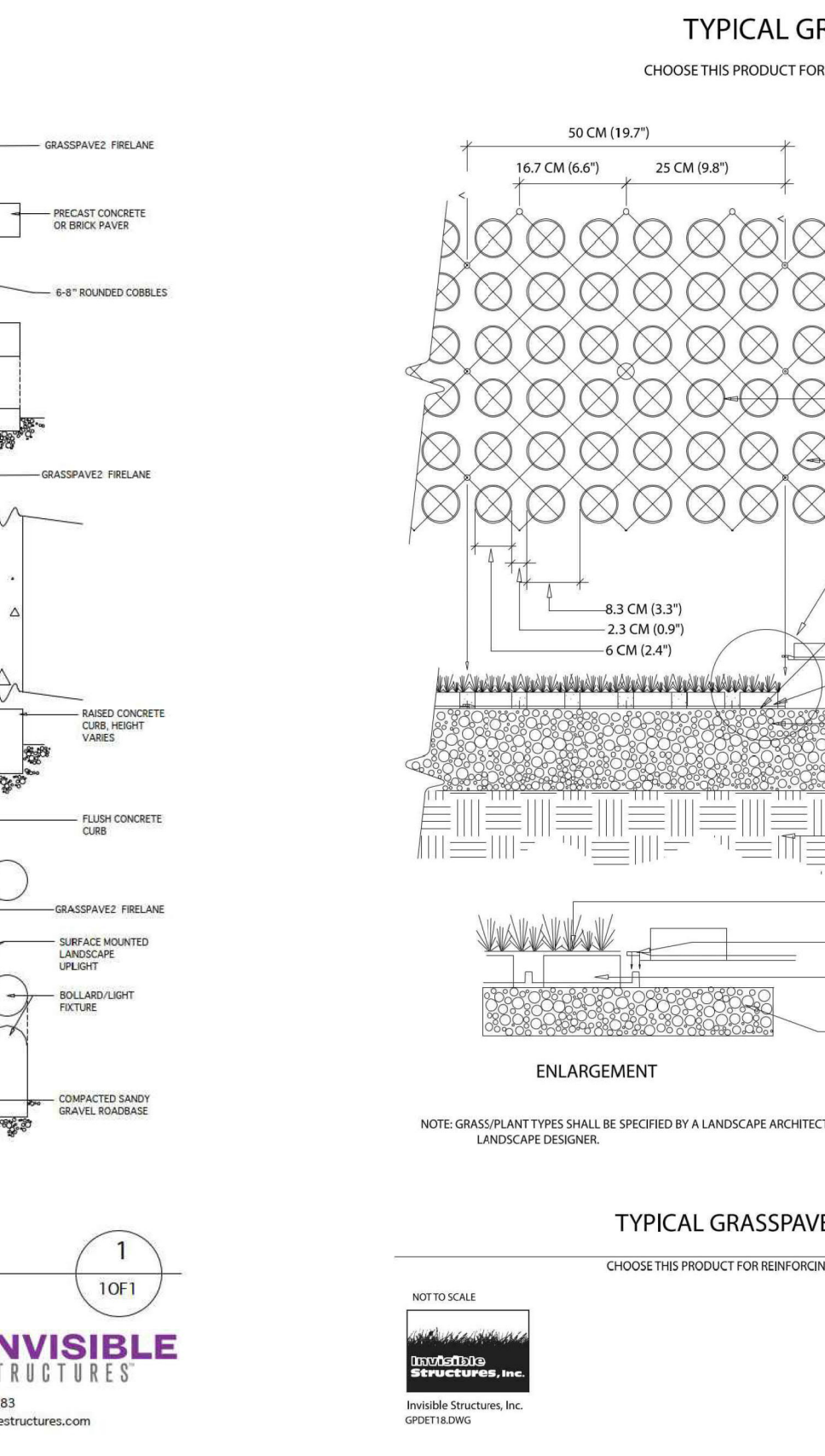
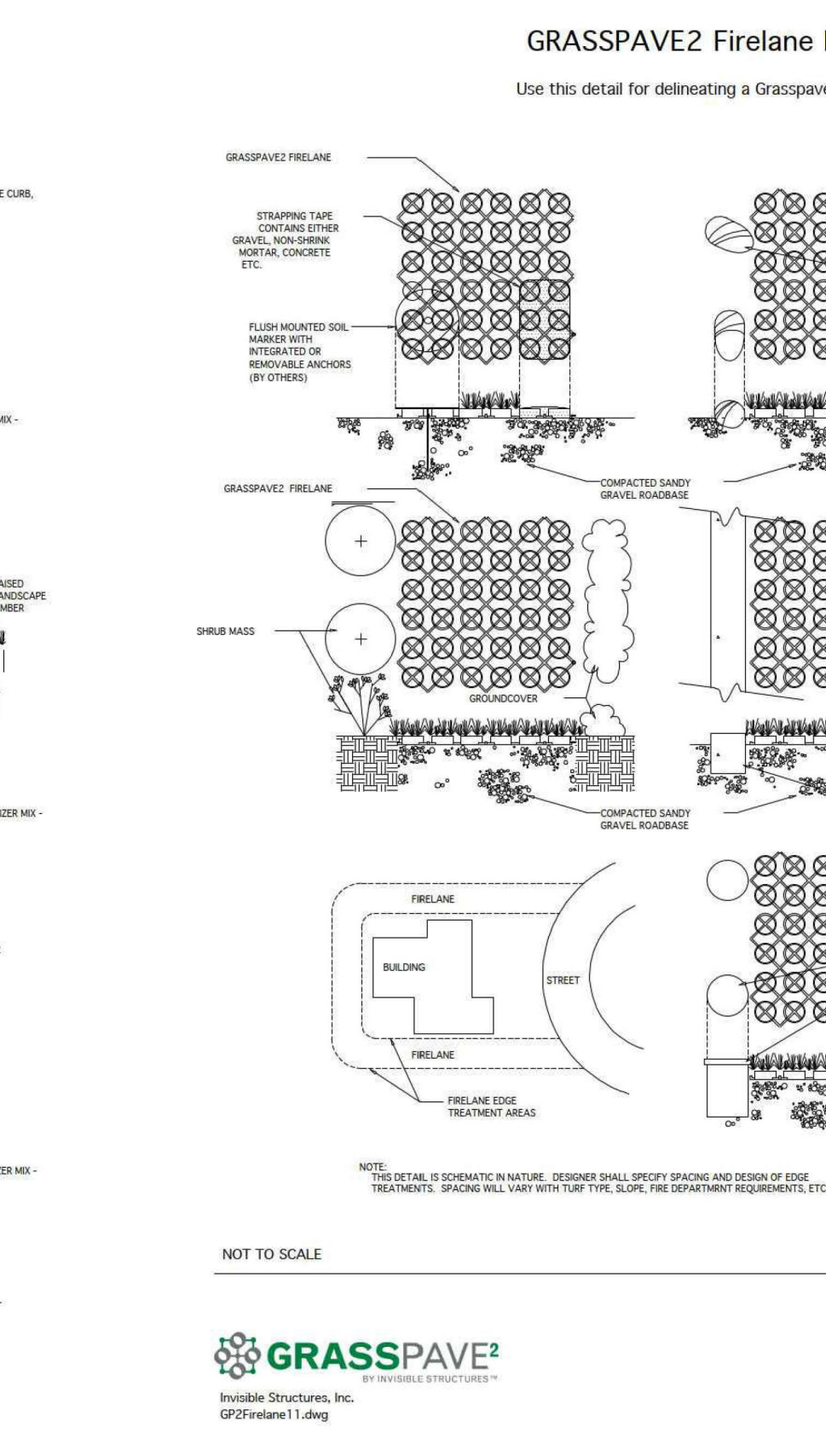
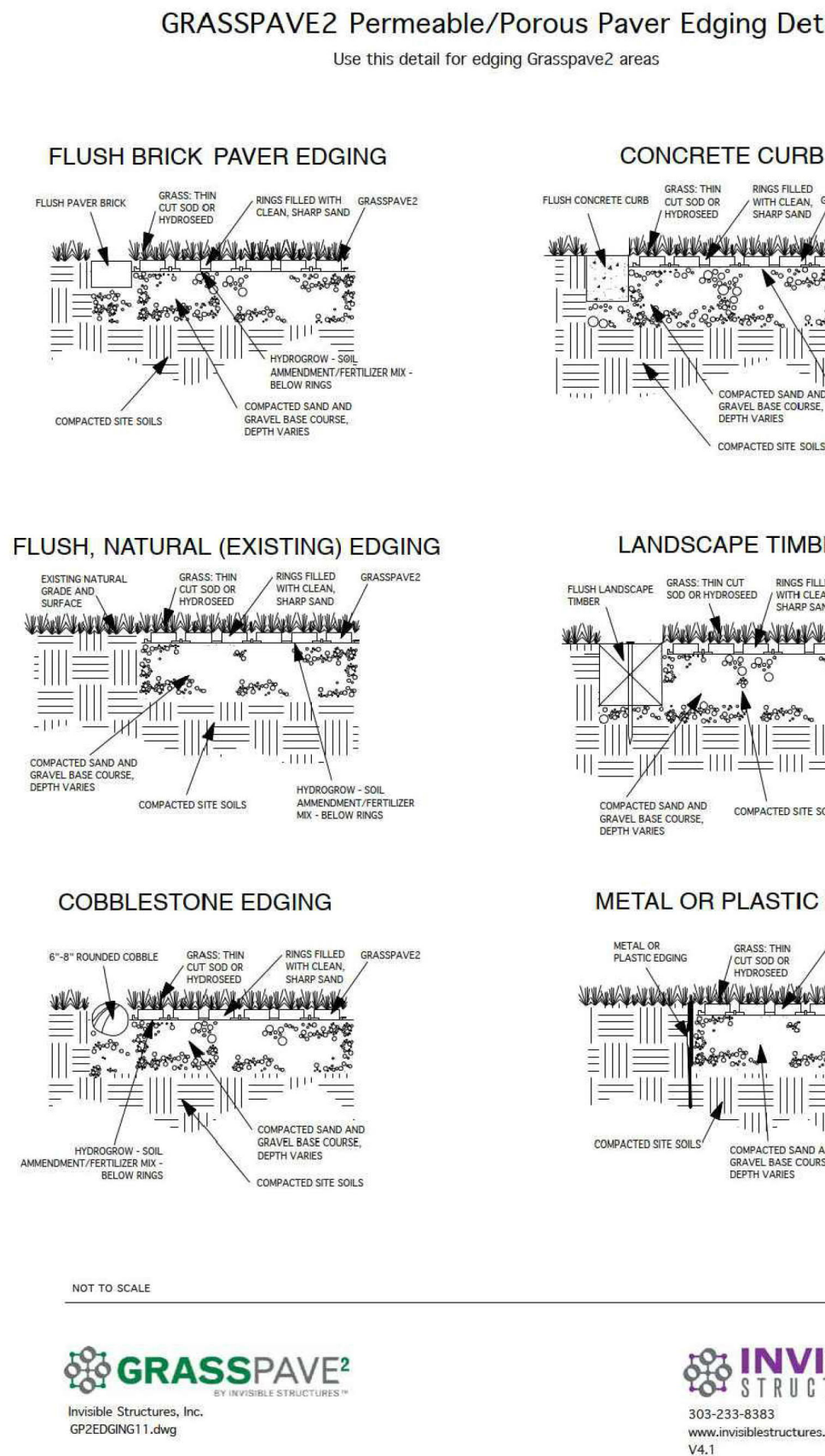
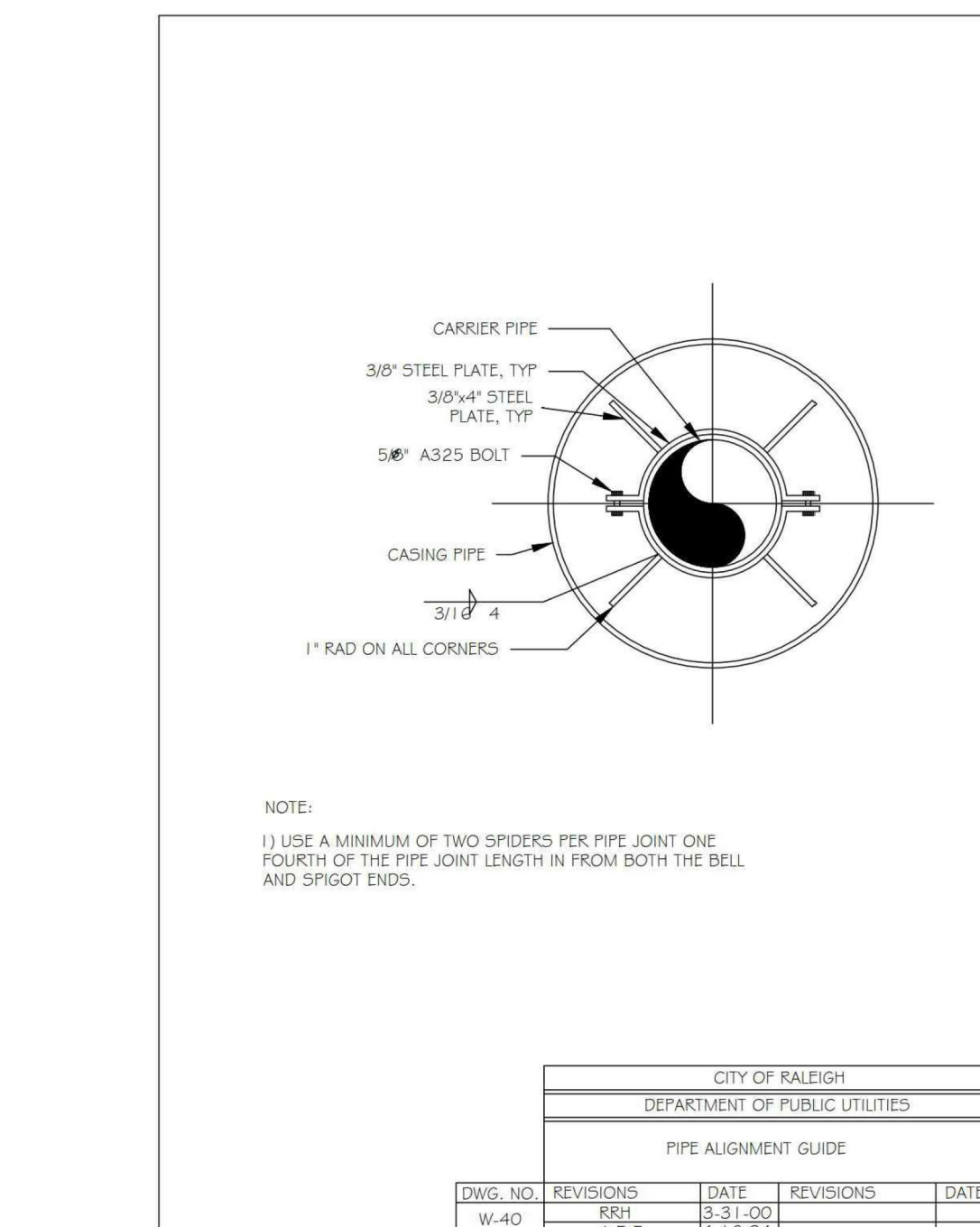
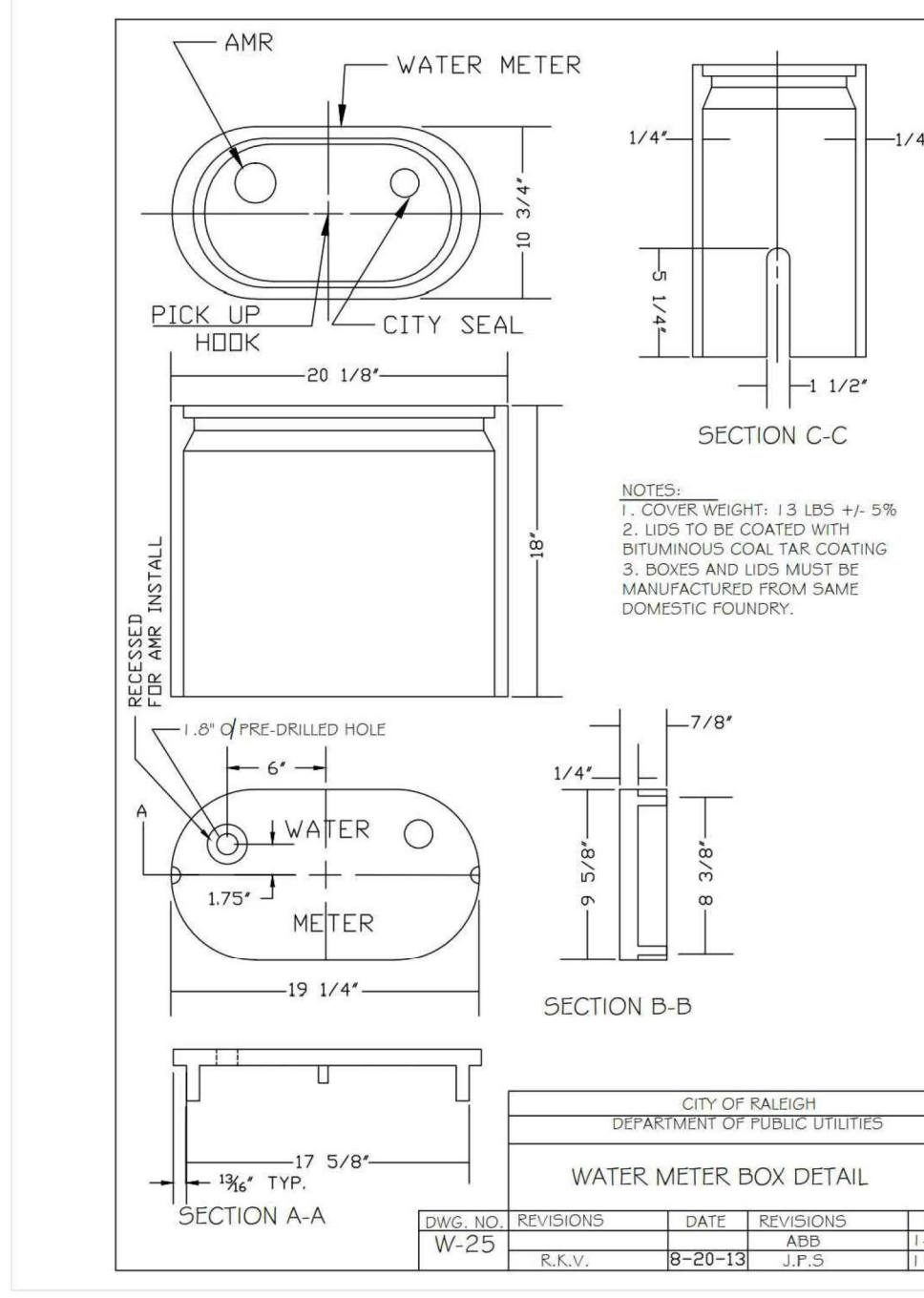
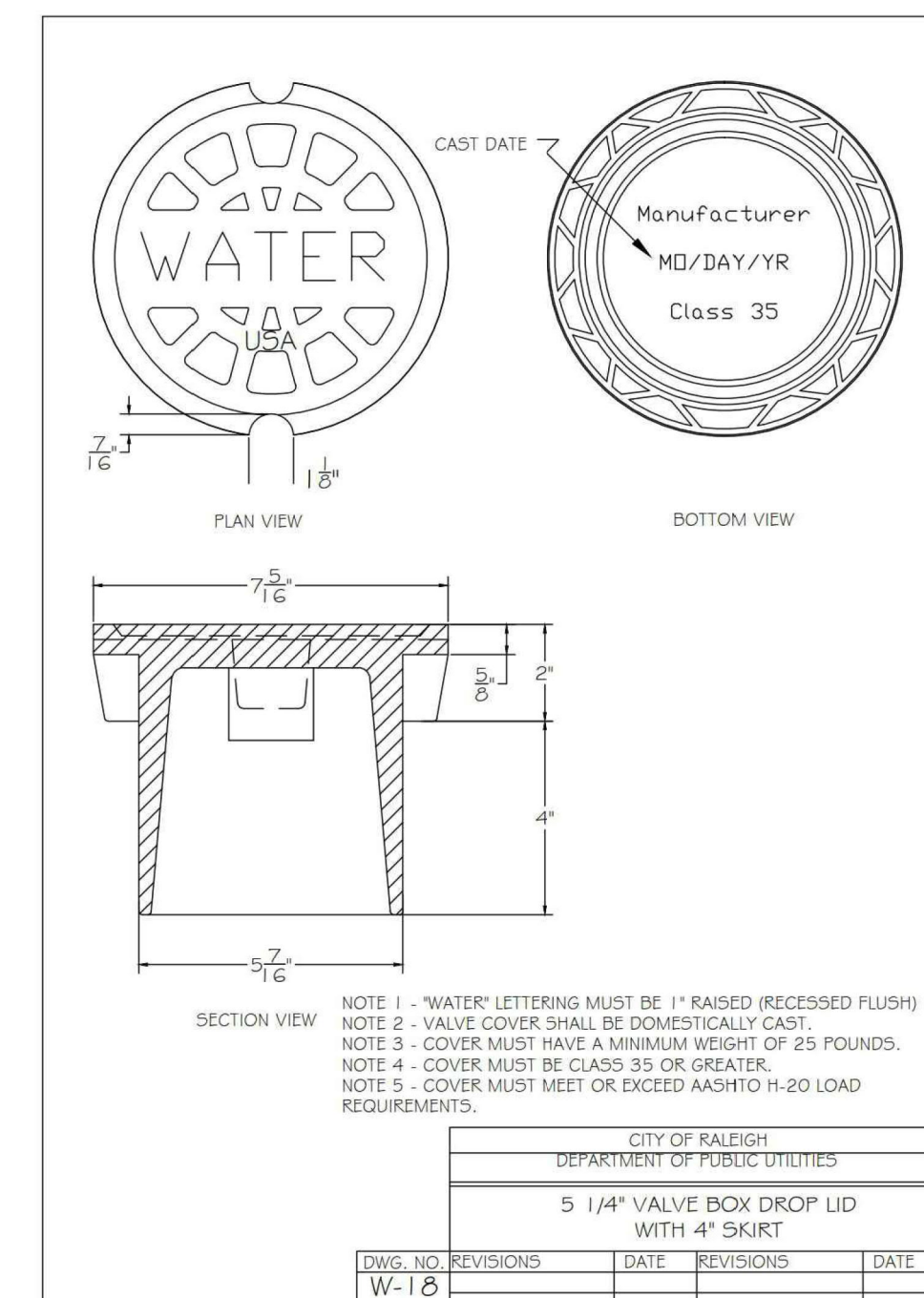
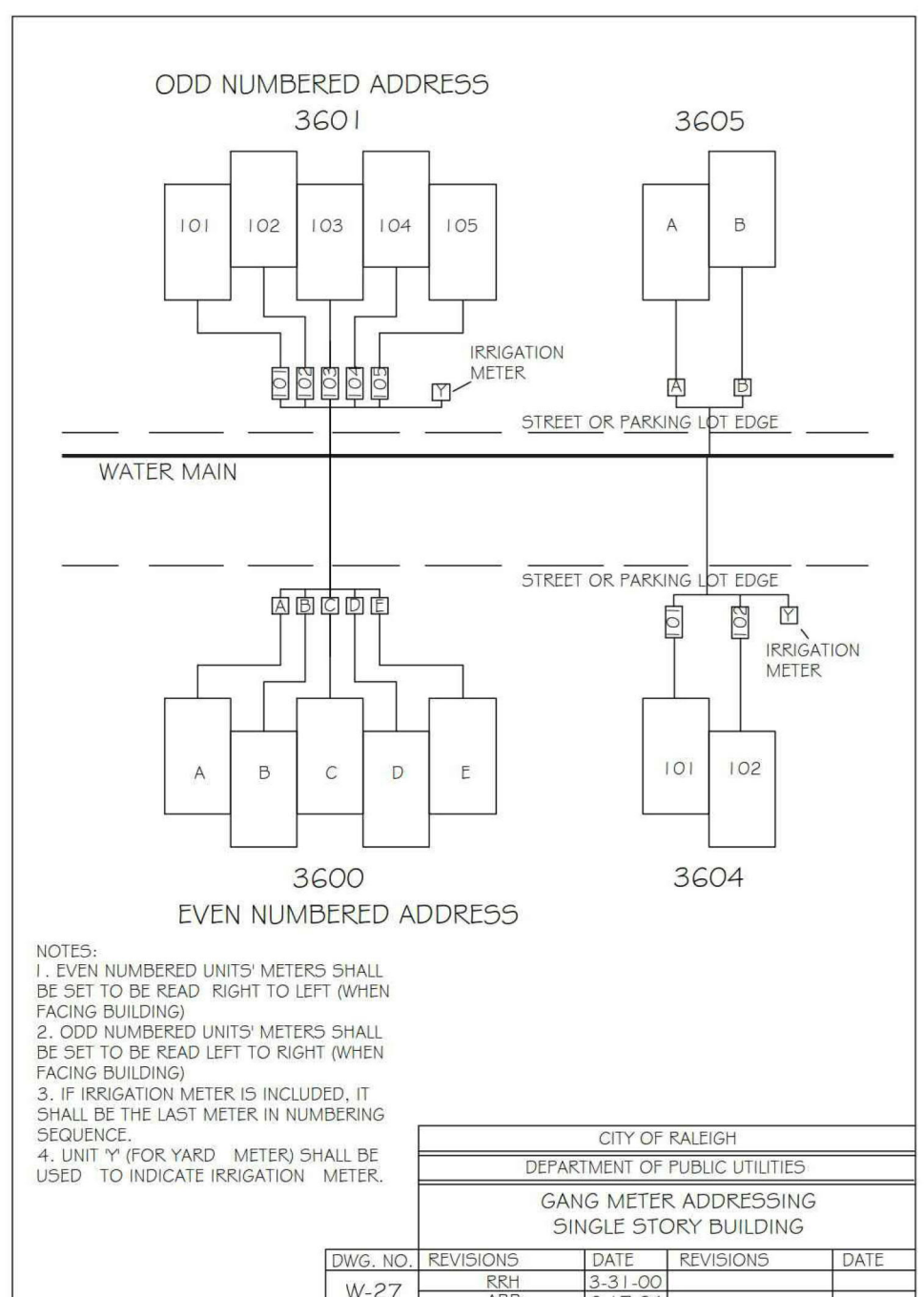
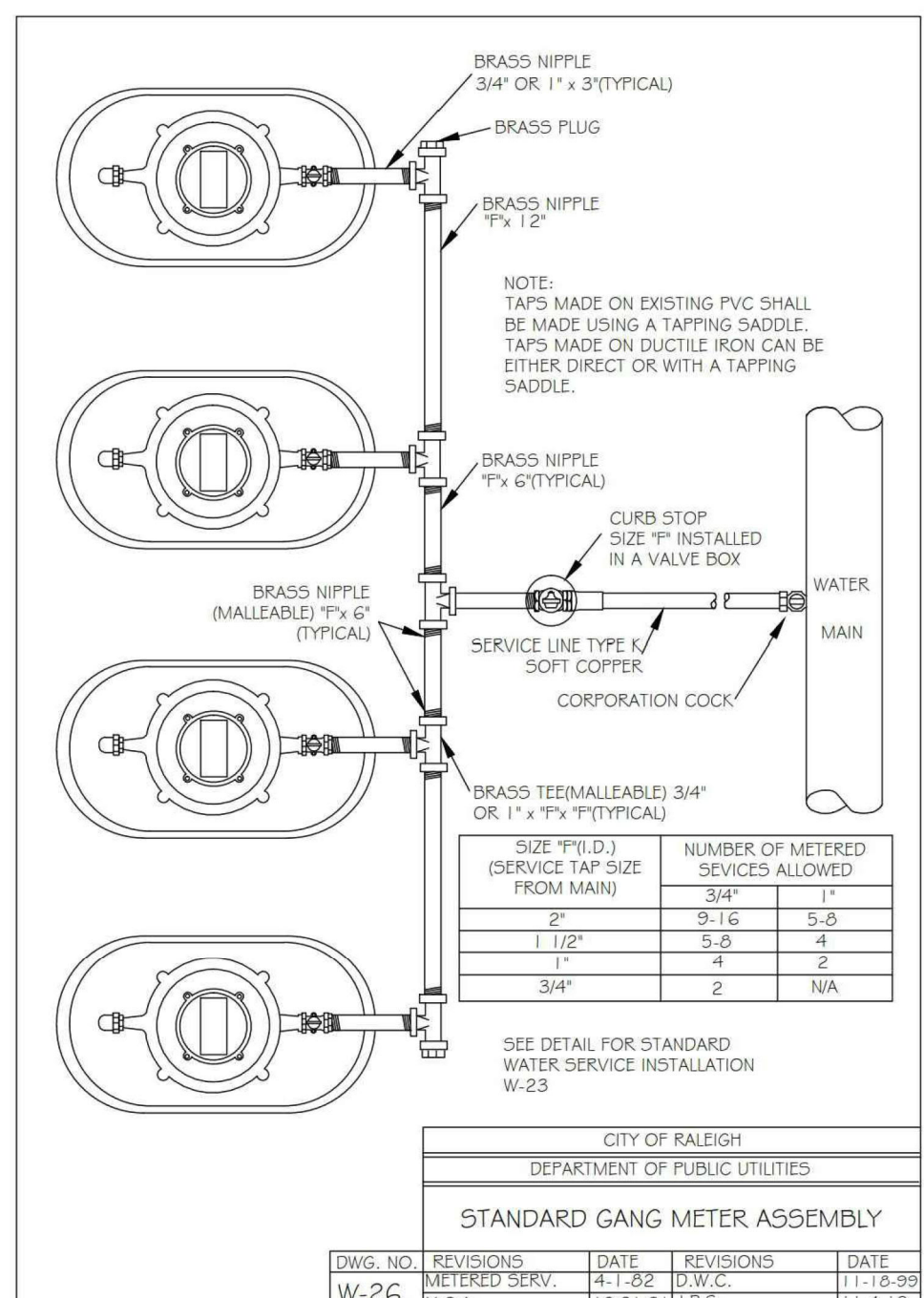
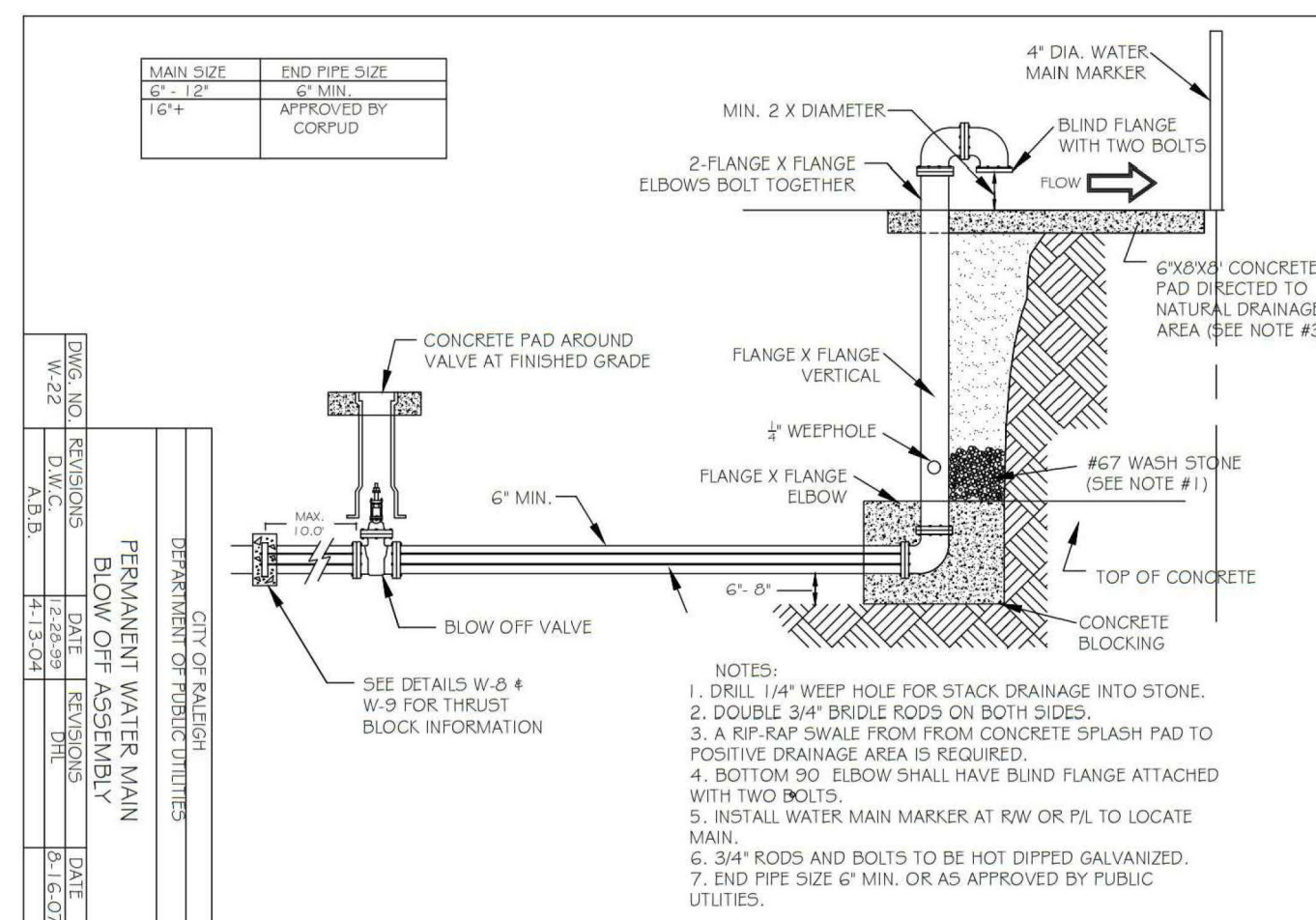
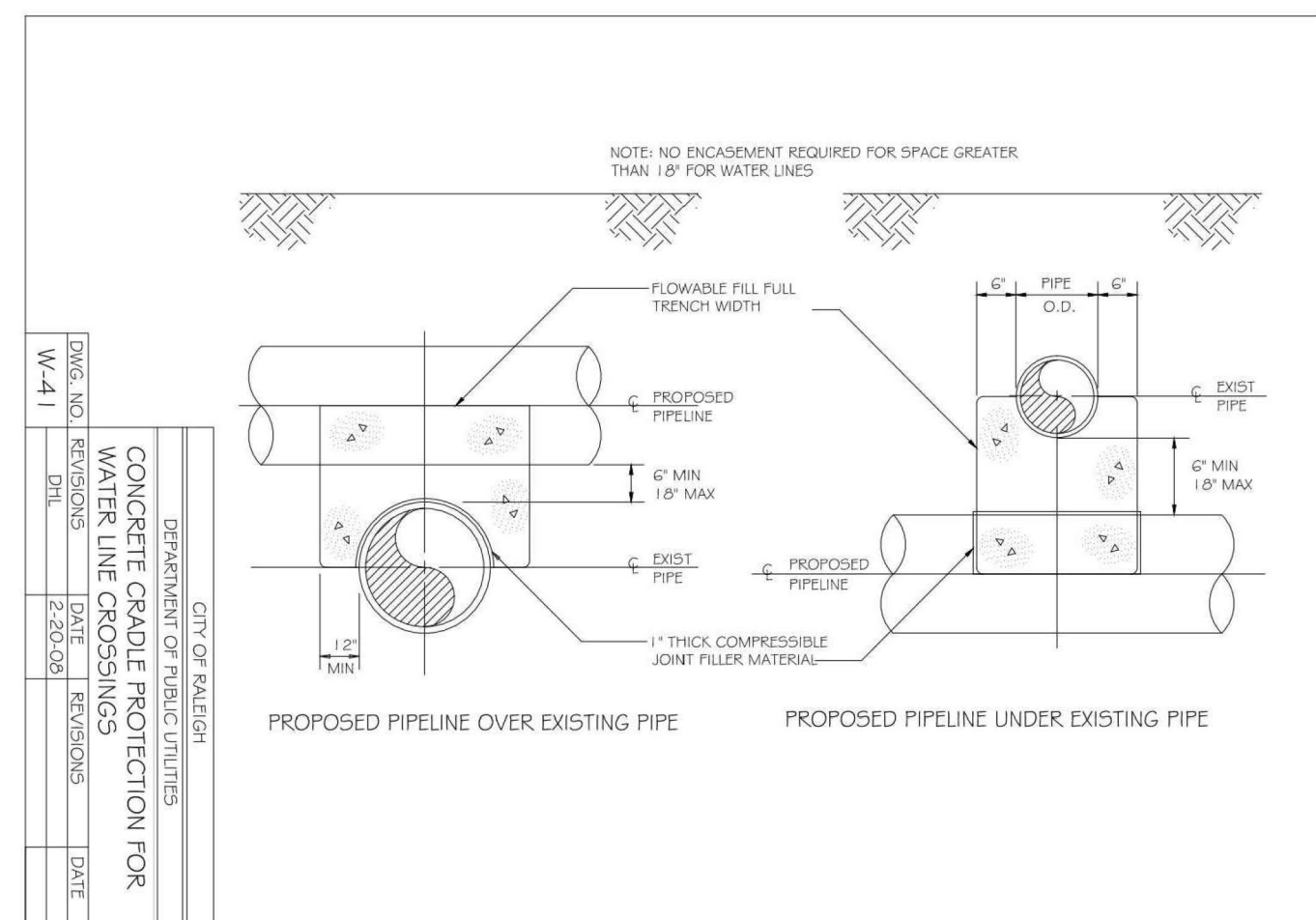
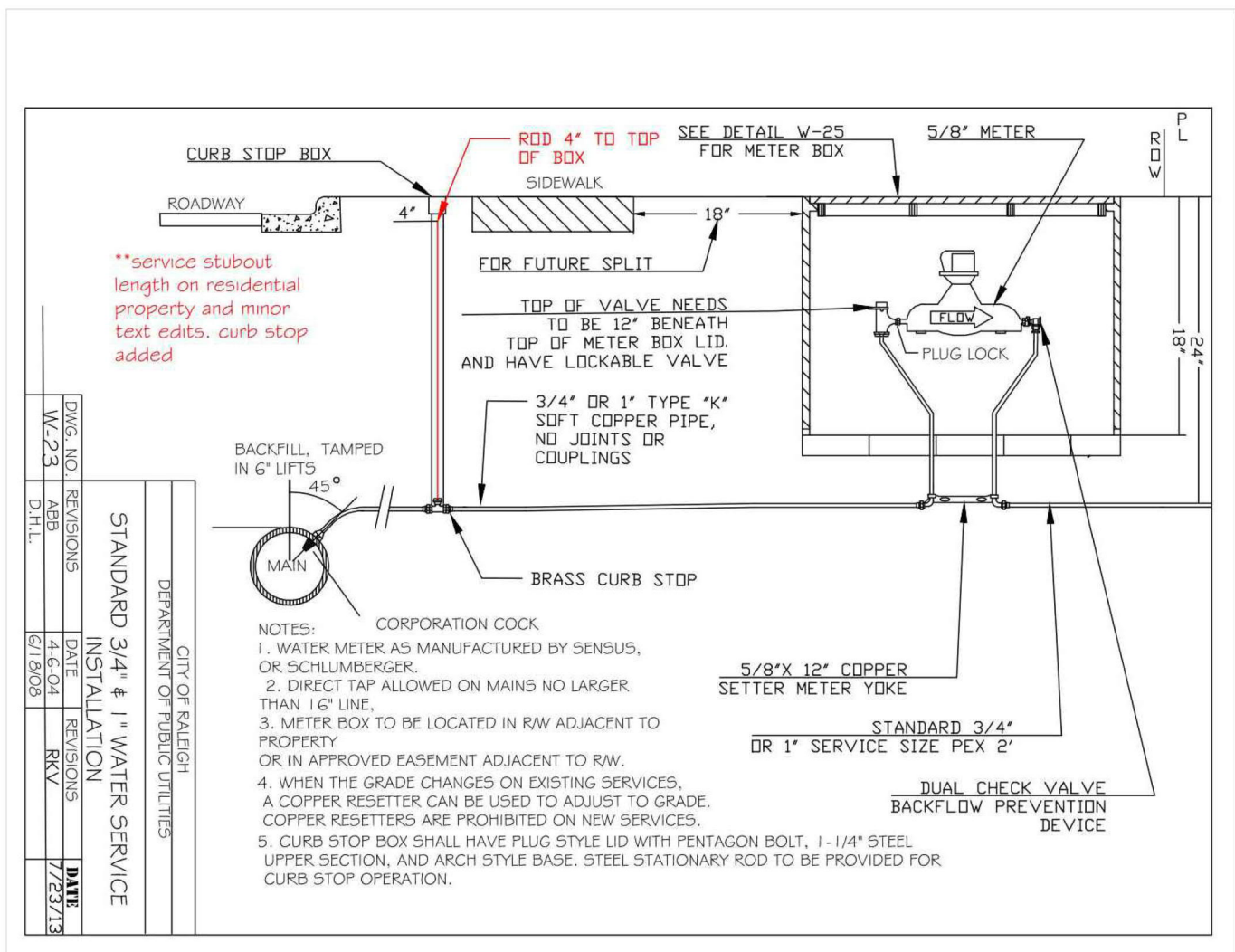
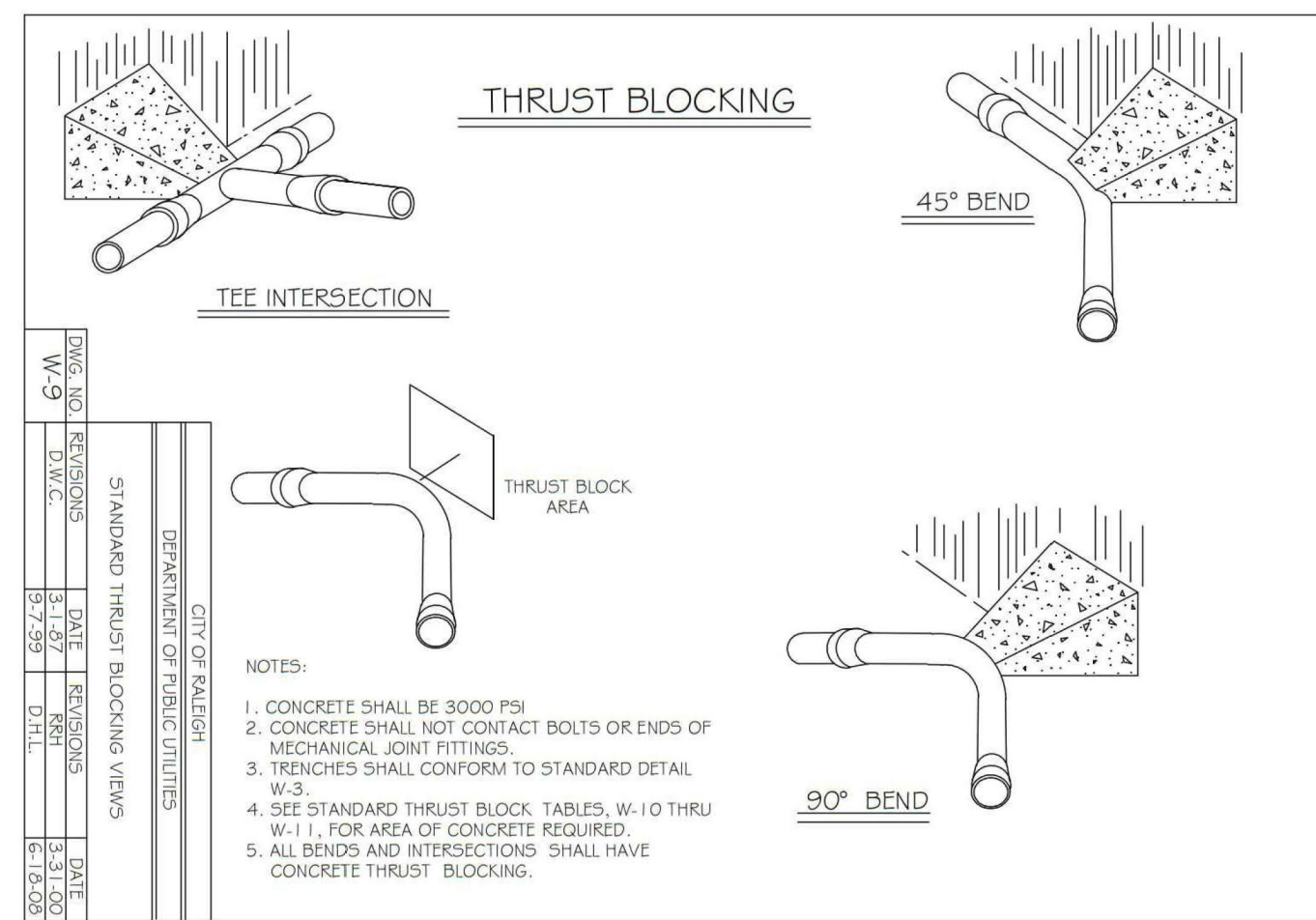


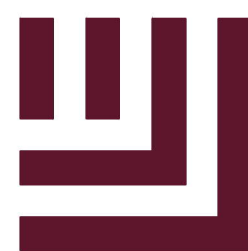
Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-14.

REACTION BEARING AREAS FOR HORIZONTAL WATER PIPE BENDS. Table with columns: Bend Angle, Pipe Size, and Reaction Area.

Table with columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Includes revision W-11.

AS:\Projects\AWH\AWH-20000\CD\Production\Engineering\Construction Drawings\AWH20000-CD-PKG-02-D1.dwg, 5/4/2023 1:32:11 PM, Sherrill Williams





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CONTACT: BOB MISHLER



ASHTON WOODS

THE POINT
PHASES 3, 4, 5, 7 AND 10
CONSTRUCTION DRAWINGS - PACKAGE 2
EAST YOUNG STREET
TOWN OF ROLESVILLE, WAKE FOREST TOWNSHIP,
WAKE COUNTY, NORTH CAROLINA

CD 20-08



William T. O'Daniel
Professional Engineer
No. 22630
State of North Carolina
w.t.o.daniel@mcadamsco.com
2023.05.05 15:27:41 -0400

REVISIONS

NO. DATE

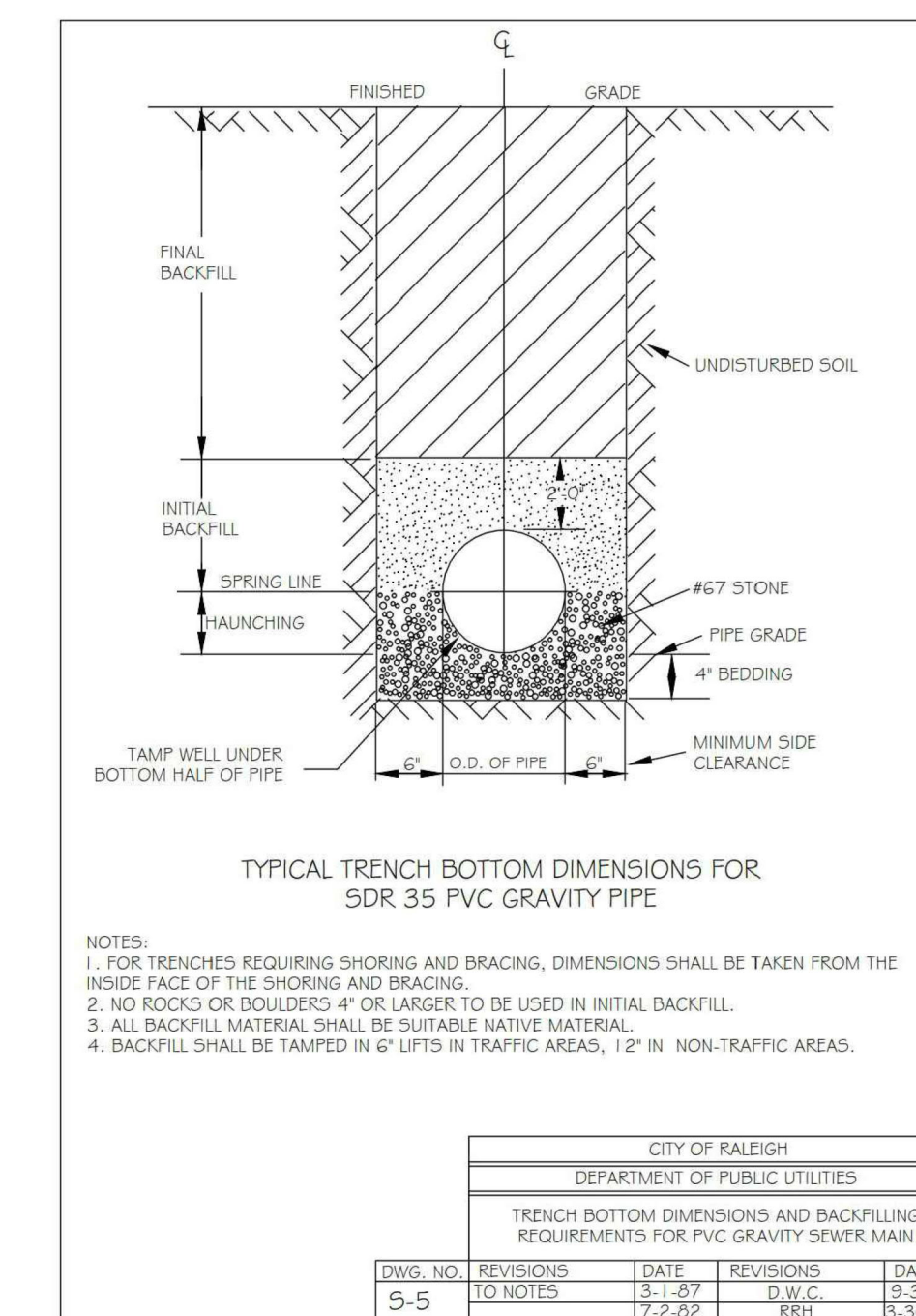
PLAN INFORMATION

PROJECT NO. AWH-20000
FILENAME AWH20000-CD-PKG-02-D1
CHECKED BY
DRAWN BY
SCALE N.T.S.
DATE 05.08.2023

SHEET

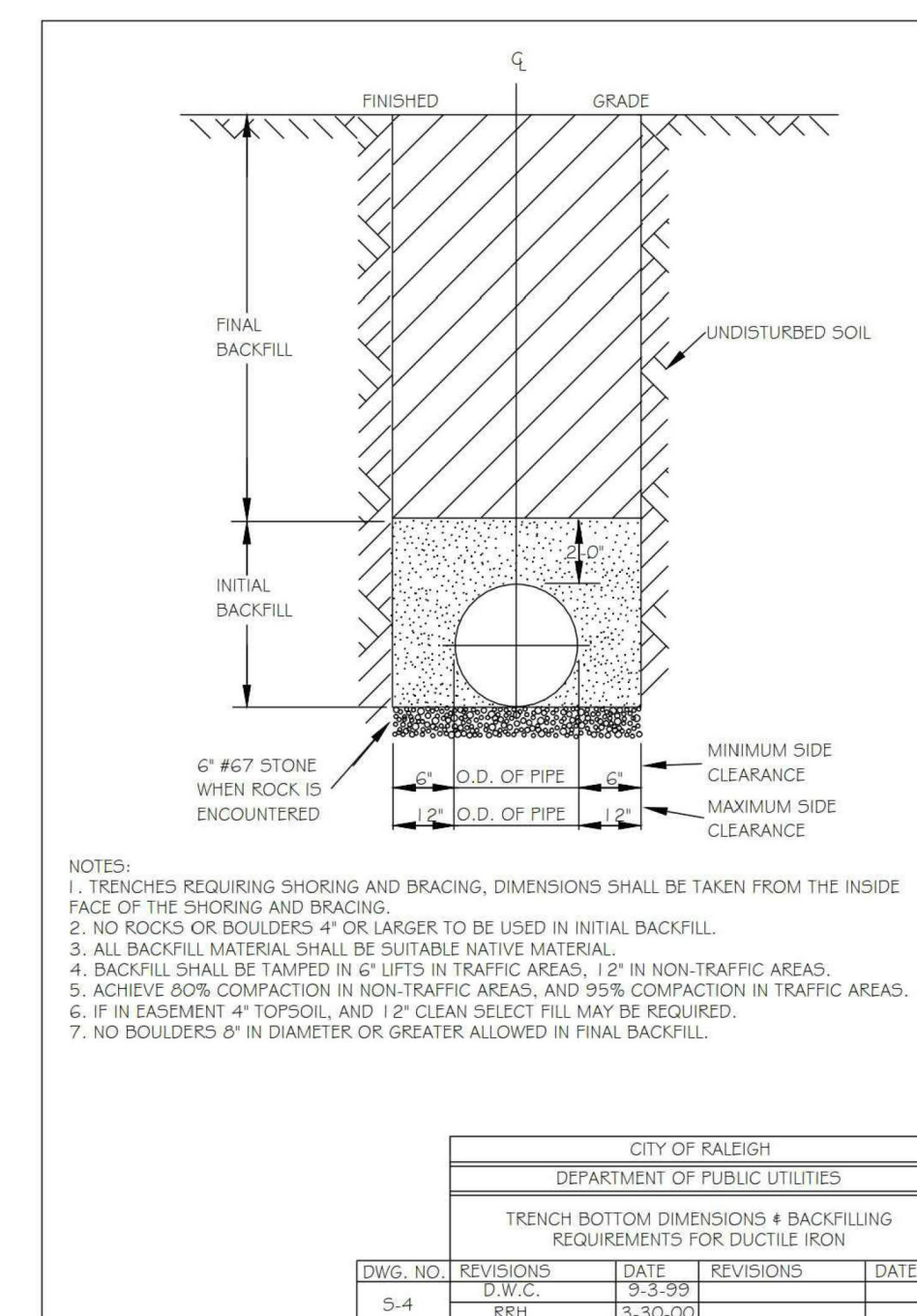
SANITARY SEWER
DETAILS

C8.06



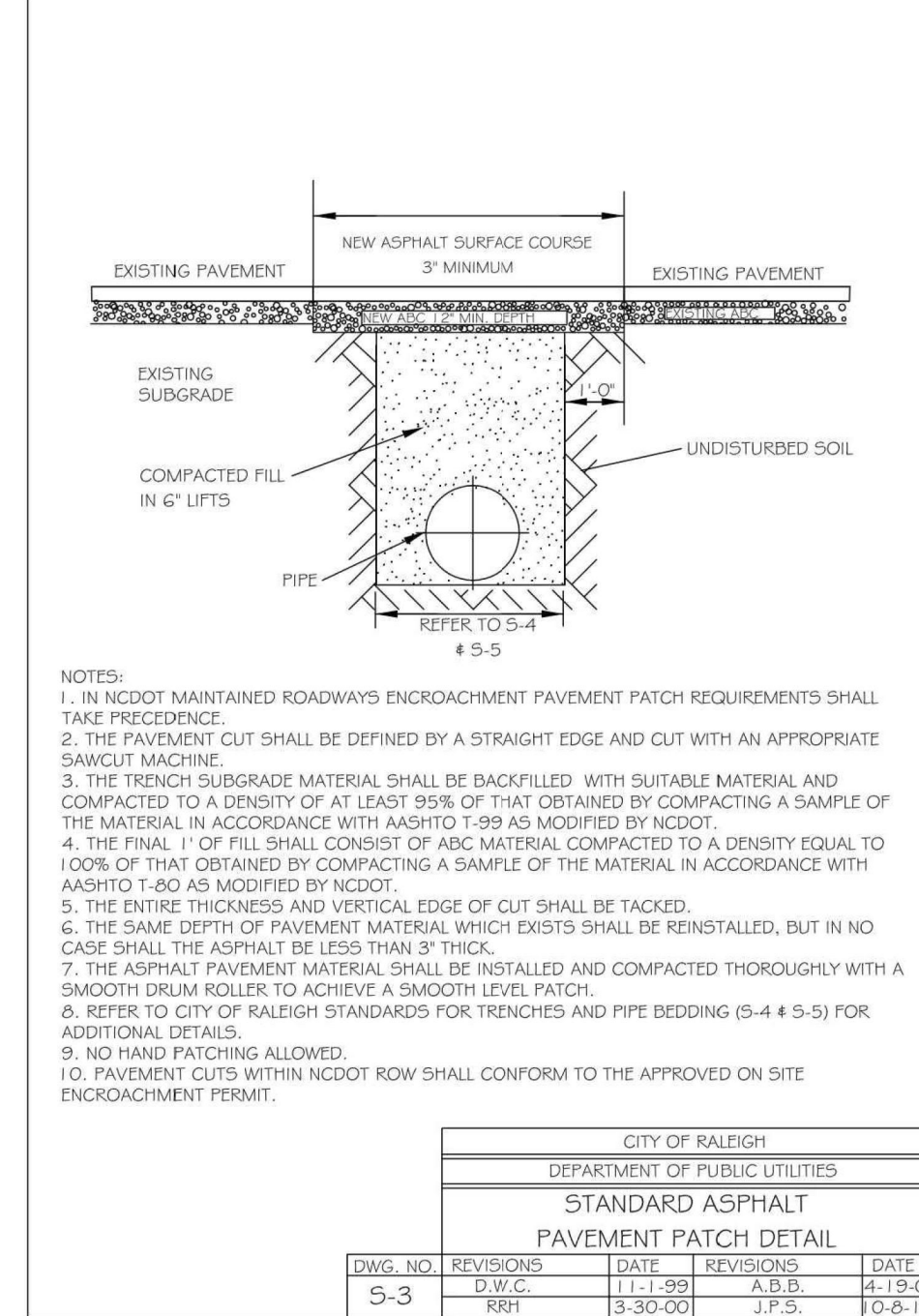
TYPICAL TRENCH BOTTOM DIMENSIONS FOR SDR 35 PVC GRAVITY PIPE

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-5, TO NOTES, 3-1-87, T.W.C., 8-3-99. Row 2: RRT, 3-30-00, RRT, 12-2-26, RRT, 3-30-00.



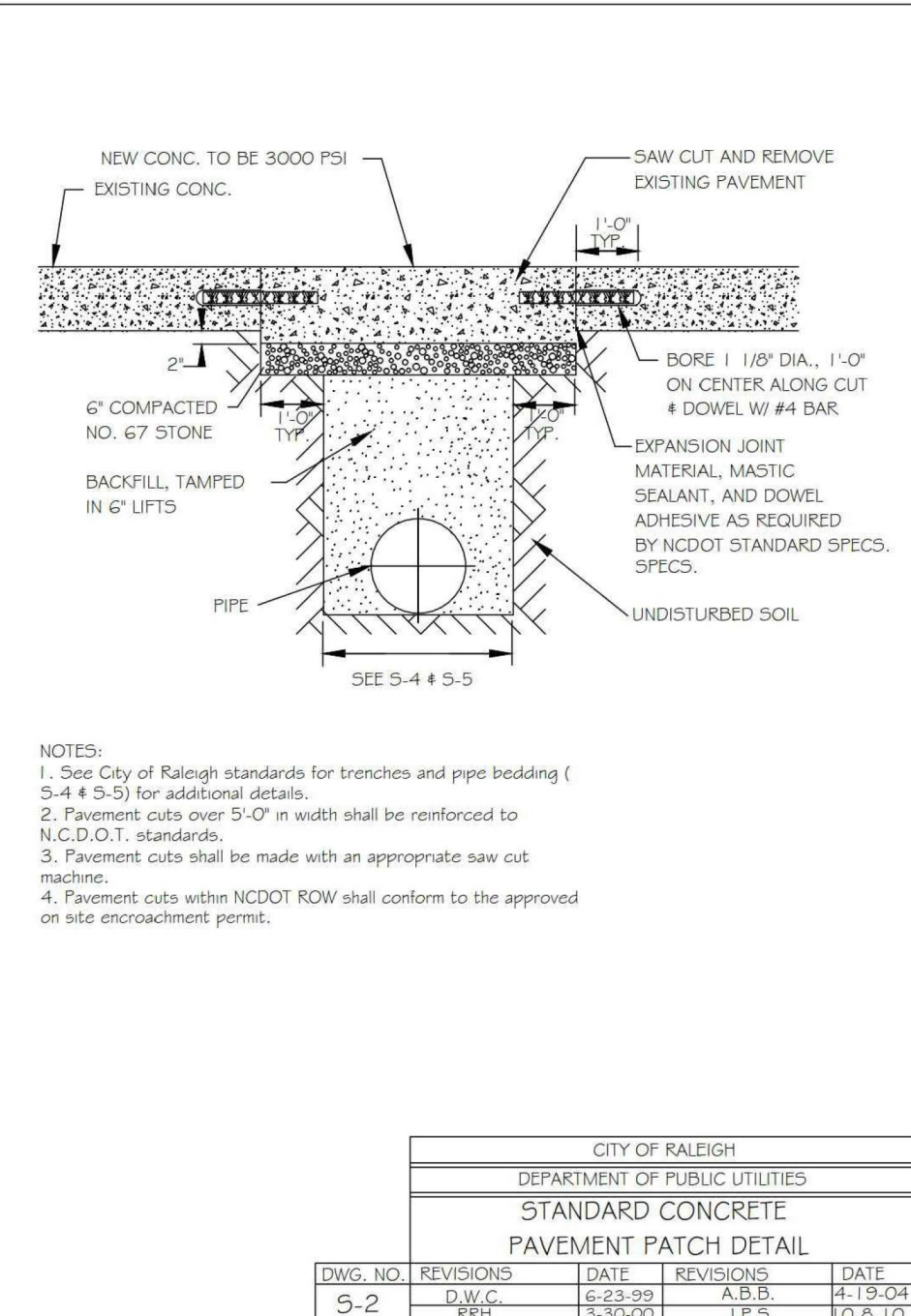
TRENCH BOTTOM DIMENSIONS & BACKFILLING REQUIREMENTS FOR DUCTILE IRON

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-4, D.W.C., 9-3-99, A.B.B., 8-19-04. Row 2: RRT, 3-30-00, J.P.S., 10-8-10.



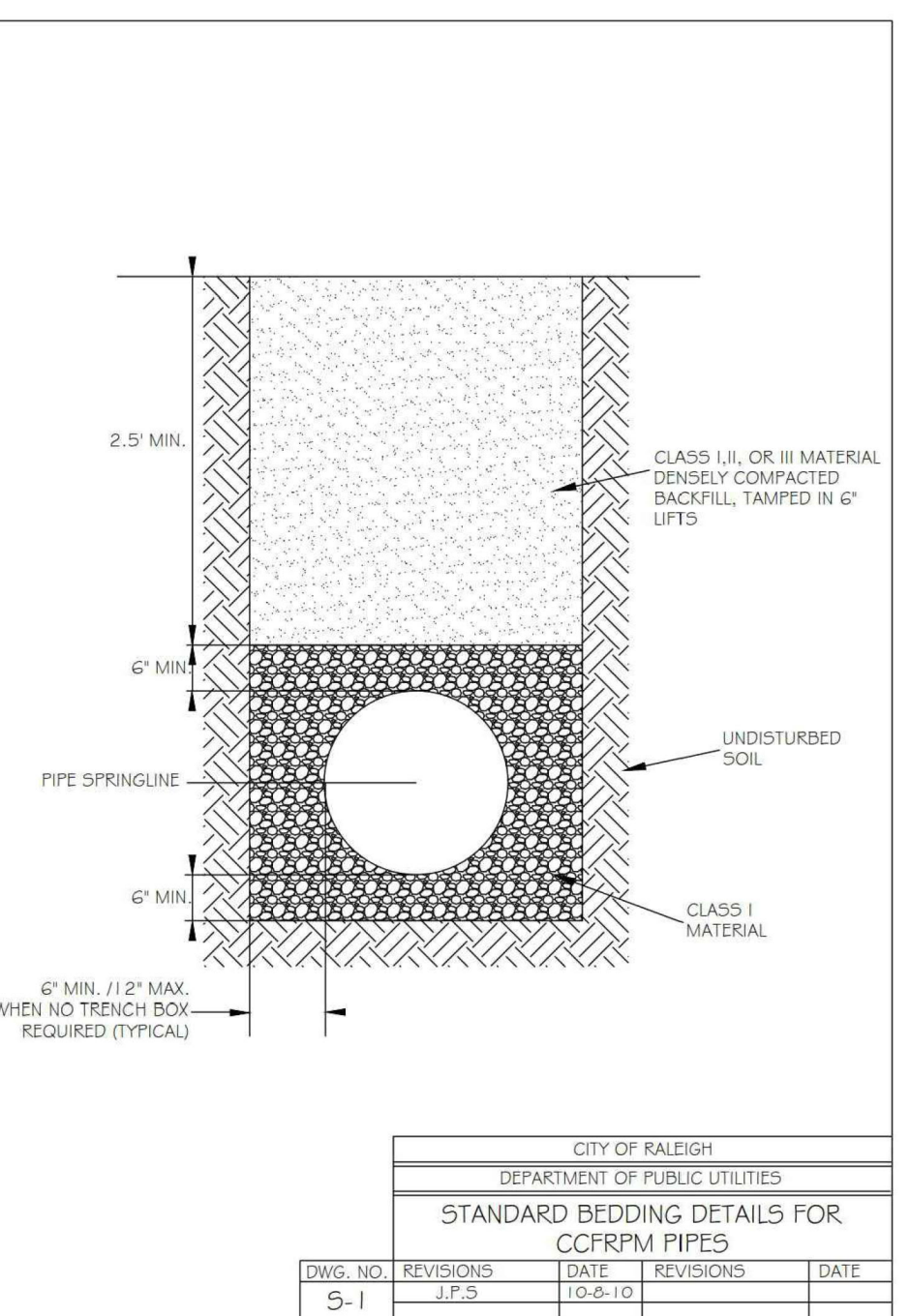
STANDARD ASPHALT PAVEMENT PATCH DETAIL

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-3, D.W.C., 1-11-99, A.B.B., 8-19-04. Row 2: RRT, 3-30-00, J.P.S., 10-8-10.



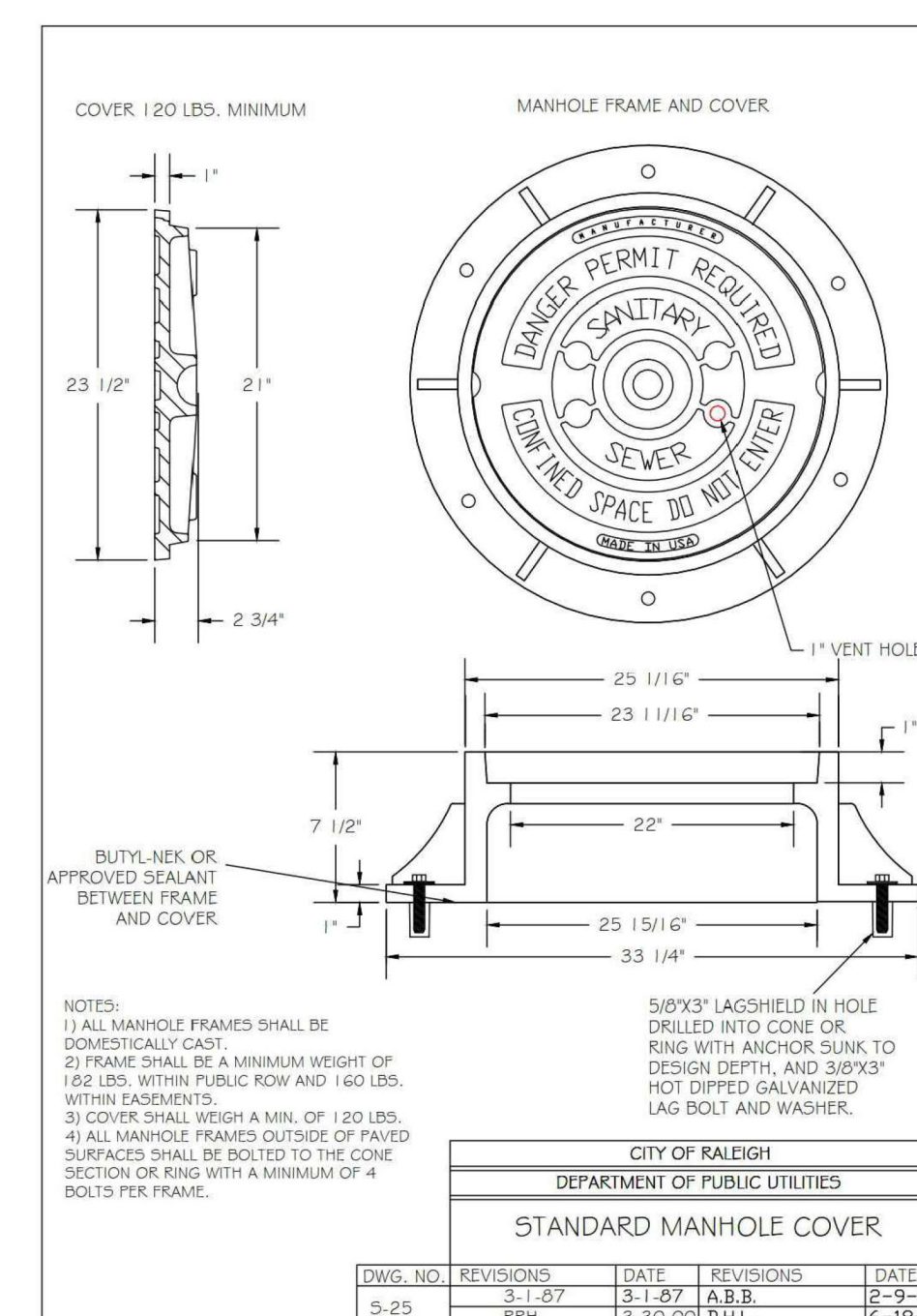
STANDARD CONCRETE PAVEMENT PATCH DETAIL

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-2, D.W.C., 6-23-99, A.B.B., 4-19-04. Row 2: RRT, 3-30-00, J.P.S., 10-8-10.



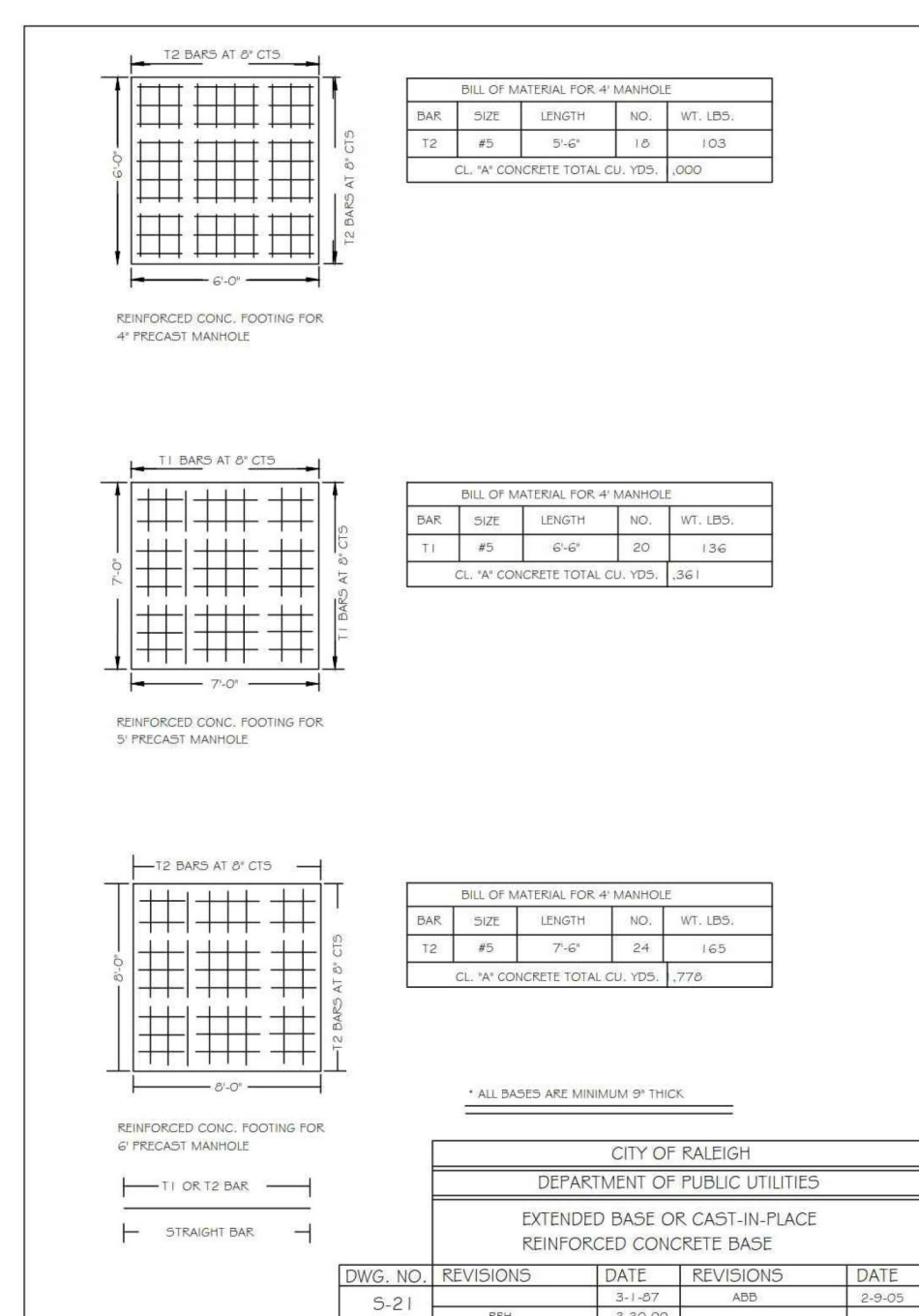
STANDARD BEDDING DETAILS FOR CCRPM PIPES

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-1, J.P.S., 10-8-10.



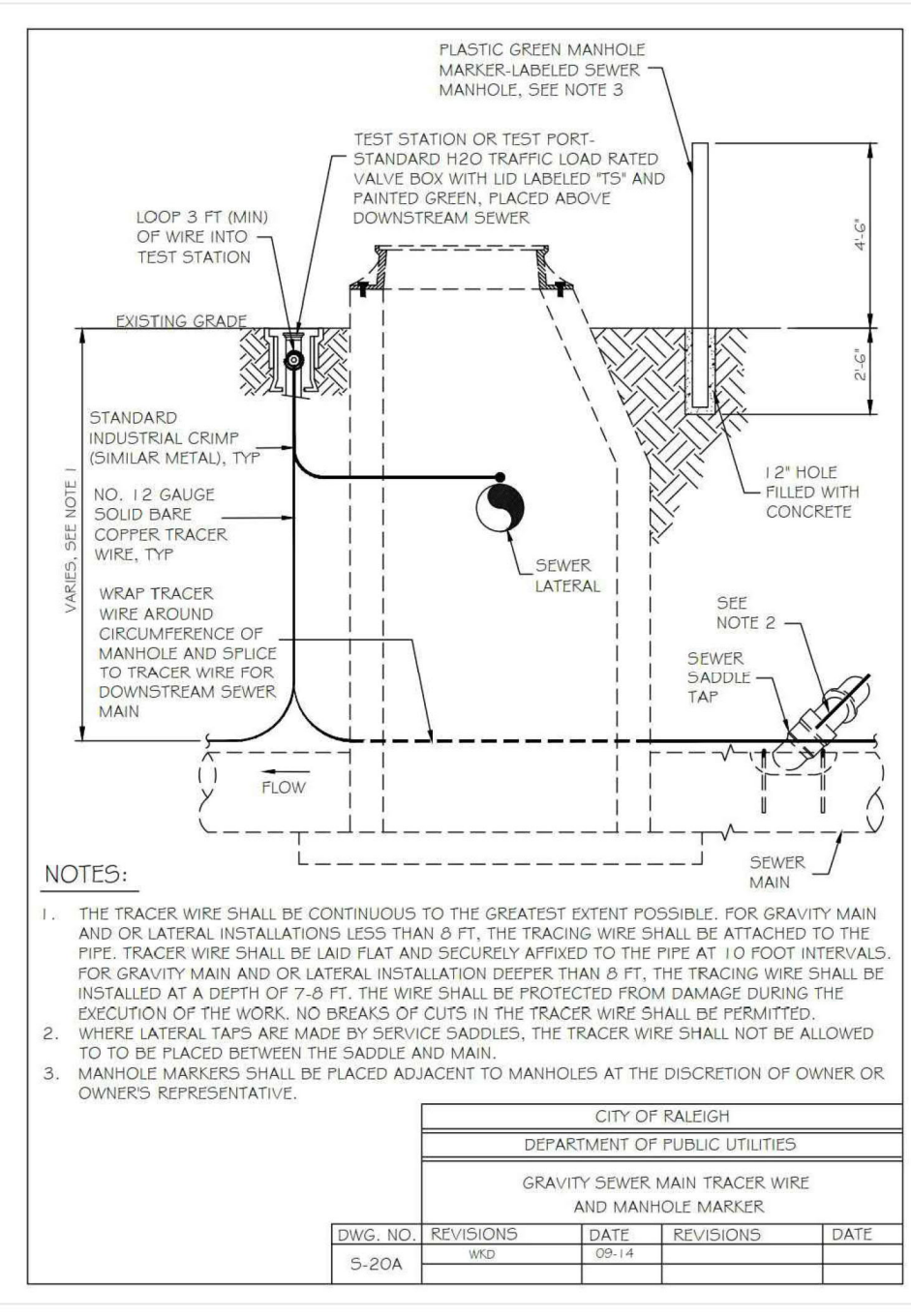
STANDARD MANHOLE COVER

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-23, S-1-97, 3-1-97, A.B.B., 2-9-05. Row 2: RRT, 3-30-00, R.H.L., 6-18-08.



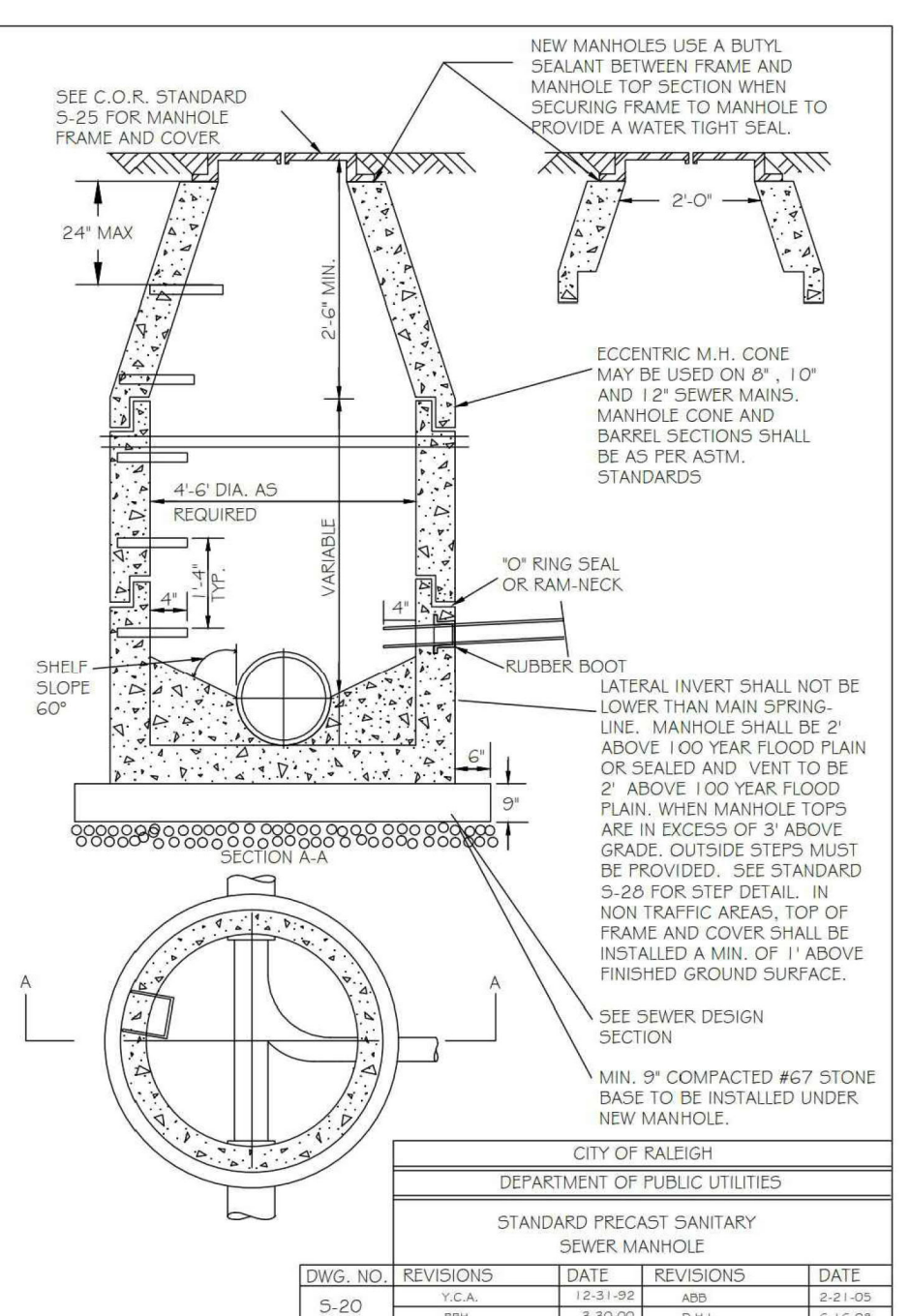
REINFORCED CONCRETE FOOTING FOR PRECAST MANHOLE

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-2, S-1-97, 3-1-97, A.B.B., 2-9-05. Row 2: RRT, 3-30-00, R.H.L., 6-18-08.



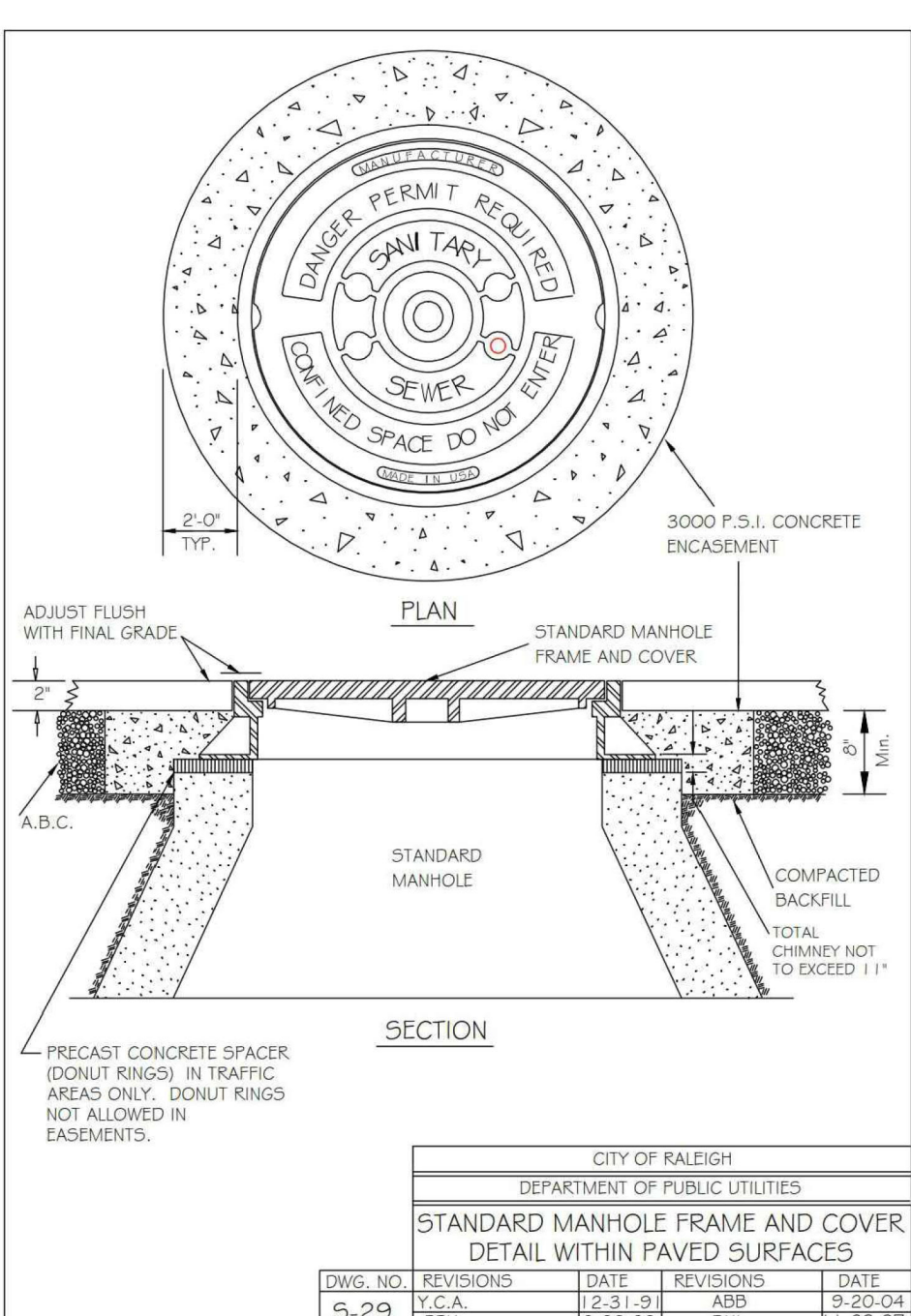
GRAVITY SEWER MAIN TRACER WIRE AND MANHOLE MARKER

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-20A, RRT, 3-30-00, D.H.L., 6-16-08.



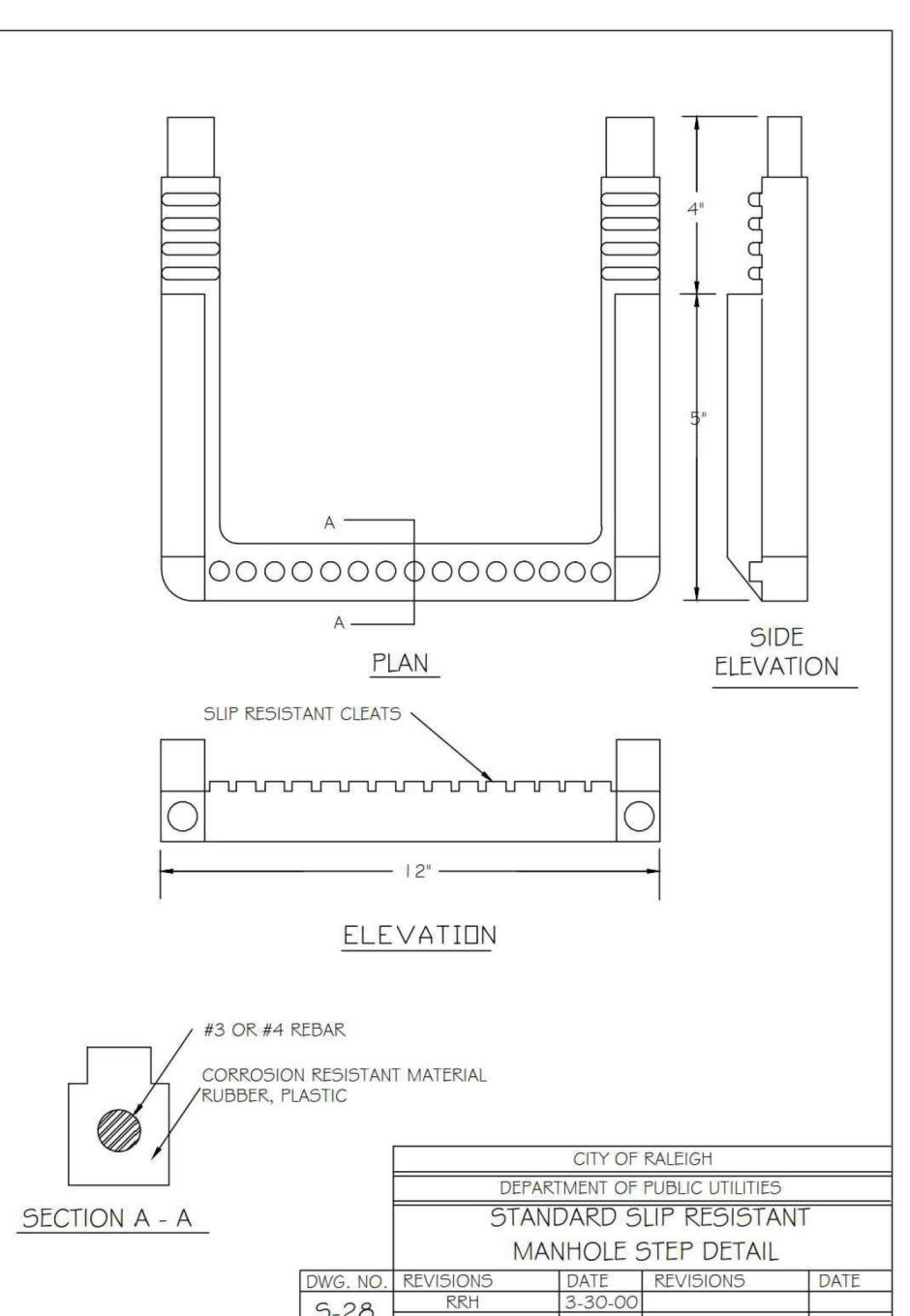
STANDARD PRECAST SANITARY SEWER MANHOLE

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-20, V.C.A., 3-31-99, A.B.B., 2-9-05. Row 2: RRT, 3-30-00, D.H.L., 6-16-08.



STANDARD MANHOLE FRAME AND COVER DETAIL WITHIN PAVED SURFACES

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-29, V.C.A., 12-31-09, A.B.B., 9-20-08. Row 2: RRT, 3-30-00, D.H.L., 11-22-07.



STANDARD SLIP RESISTANT MANHOLE STEP DETAIL

Table with 5 columns: DWG. NO., REVISIONS, DATE, REVISIONS, DATE. Row 1: S-28, RRT, 3-30-00.

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